

LONDON BOROUGH OF ENFIELD

PLANNING COMMITTEE

Date: 23 January 2024

Report of

Director of Planning & Growth - Brett Leahy

Contact Officers:

Allison Russell - Case Officer
Karolina Grebowiec-Hall –
Planning Decisions Manager

Category

Major

Ward

Upper Edmonton

Councillor Request

No

LOCATION: Land at Commercial Road And 4 And 8 Bull Lane N18

APPLICATION NUMBER: 22/03672/FUL

PROPOSAL: Demolition of all buildings and structures and redevelopment of the site with the erection of three buildings subdivided into seven units for B2 general industrial, B8 storage and distribution, and E(g)(iii) light industrial purposes with ancillary offices, together with associated car parking, servicing and landscape planting.

Applicant Name & Address:

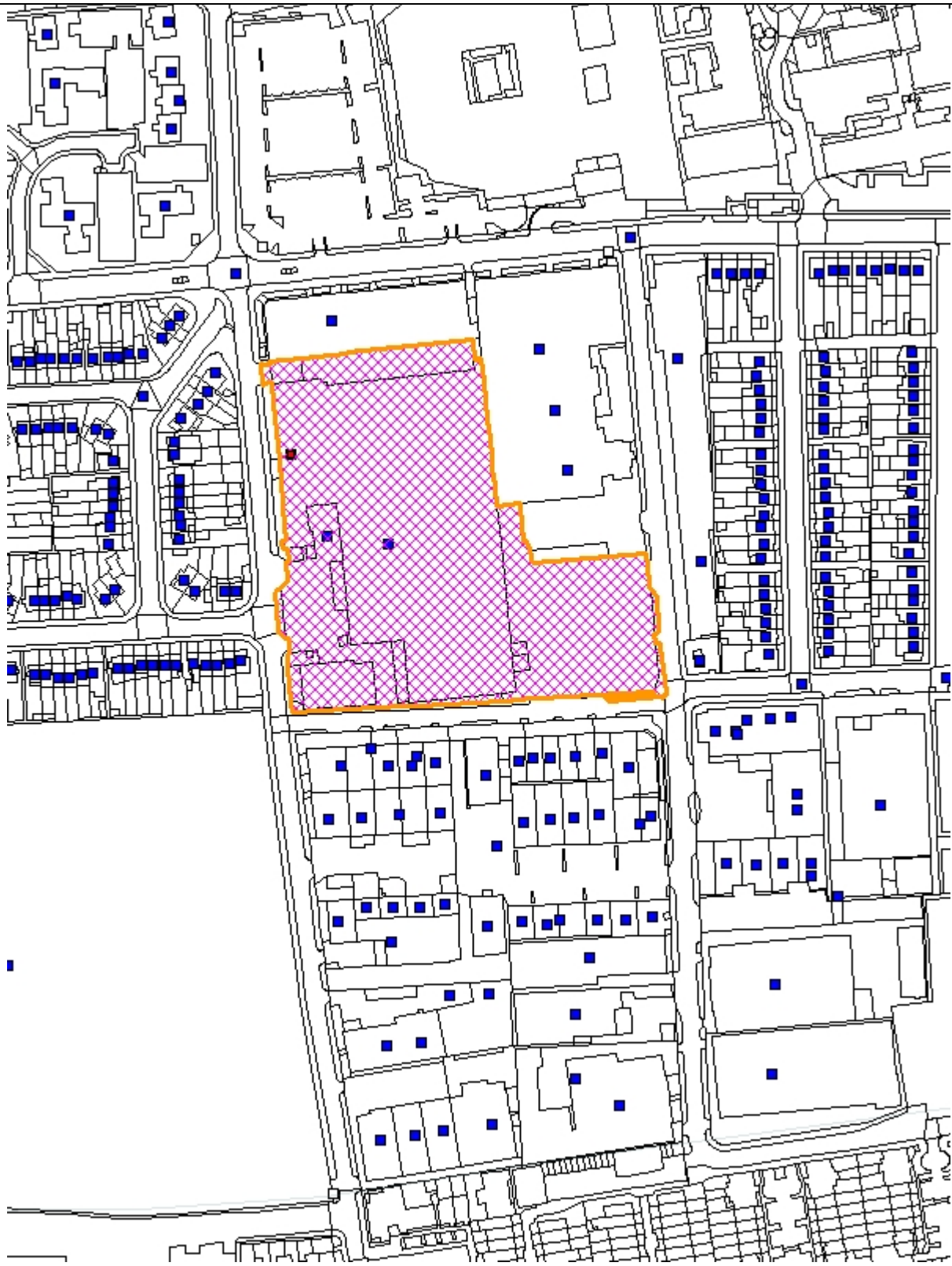
HE2 UK Enterprises 12 GP Limited

Agent Name & Address:

PRC Architecture & Planning Ltd

RECOMMENDATION:

1. That subject to the completion of a Section 106 Agreement to secure the obligations as set out in the report, the Head of Planning and Building Control be authorised to **GRANT** planning permission subject to conditions.
2. That the Head of Planning and Building Control be granted delegated authority to finalise the wording of the Section 106 Agreement and the conditions to cover the matters in the Recommendation section of this report.



1. Note for Members

- 1.1 This planning application is categorised as a 'major' planning application involving development carried out on a site having an area of 1 hectare or more on a Locally Significant Industrial Site (LSIL). In accordance with the scheme of delegation it is reported to Planning Committee for determination.

2. Recommendation

- 2.1 That subject to the completion of a Section 106 Agreement to secure the obligations as set out in the report, the Head of Planning and Building Control be authorised to GRANT planning permission subject to conditions:
1. Time limit
 2. Approved plans
 3. Restricted uses class – Use Classes B2, B8 and E(g)(iii) only
 4. Detailed drawings and sample materials
 5. Landscape Management and Monitoring Plan, including BNG
 6. Details of green roof and living wall
 7. Final sustainable drainage strategy
 8. Drainage strategy verification report
 9. Energy Statement compliance and energy performance certificates, including monitoring PV performance
 10. BREEAM assessment Excellent (design stage and post-construction)
 11. Overheating Risk Assessment
 12. EV charging details
 13. Cycle parking details
 14. Construction Management Plan (for transport and dust emissions)
 15. Delivery and servicing plan
 16. Parking bay restriction
 17. Land contamination remediation scheme
 18. Final noise impact assessment
 19. Secured by Design
 20. External lighting report compliance
 21. Clearance during nesting bird season
 22. Bat boxes
 23. Bird boxes
 24. Fire statement compliance
 25. Site Waste Management Plan – construction
 26. Site Waste Management Plan – operational
 27. Non Road Mobile Machinery - NRMM
 28. Health Impact Assessment compliance
- 2.2 That the Head of Planning and Building Control be granted delegated authority to finalise the wording of the Section 106 Agreement and the conditions to cover the matters in the recommendation section of this report.

3. Executive Summary

- 3.1. The proposed development is for the erection of new commercial development comprising three buildings subdivided into seven units on an L-shaped site in Edmonton. The development proposes to retain industrial use of the site on this designated Locally Significant Industrial Site (LSIL) with B2 general industrial, B8

storage and distribution, and E(g)(iii) light industrial purposes with ancillary offices.

- 3.2. The existing, poor and dated condition of the site is proposed to be improved with modern, well-designed buildings on a site that is landscaped and arranged to provide the vehicle, bicycle and HGV parking that is essential to the operation of the industrial units.
- 3.3. The principle of the development is strongly supported, as it retains and expands industrial use in this LSIL. The upgrading of the site also introduces the potential for increased employment and opportunities for a larger range of businesses. The applicant has worked with officers to enhance the design of the proposal. Officers support the massing, layout and architecture of the project. The planting of trees and broader landscape strategy are supported. The applicant has addressed officers' comments on sustainable urban drainage features and has proposed construction that, with a set of conditions and S106 obligations, will meet energy and sustainability requirements. The site plan has been rationalised to relieve vehicle movements from Bull Lane and better utilise Commercial Road. In order to help control the number of trips to and from the site, a condition is recommended to require that vehicles park in parking bays and nowhere else within the site.
- 3.4. The primary public benefits of the scheme can be summarised as follows:
 - The provision of a modern, suitably designed and constructed industrial space in a designated Locally Significant Industrial Site.
 - Optimisation of the site by rectifying the site layout and providing a net increase of 345 sqm of floorspace.
 - The potential for an increase in employment on the site from the current condition.
 - Provision of seven industrial units of varying size and configuration, providing accommodation to a larger variety of businesses.
 - Improving the streetscape with more engaging frontages and integrated landscaping.
 - Reducing vehicle access on Bull Lane to make the street safer as a strategic cycle link.
 - Substantial improvement to on-site sustainable drainage.

4. Site and surroundings

- 4.1. The development proposals cover an L shaped parcel of land that is situated in the long-established employment area of Edmonton within a Locally Significant Industrial Site. The land parcel is located to the south of Bridport Lane and boarded by Bull Lane to the west, Shaftesbury Road to the south, and Commercial Road to the east.
- 4.2. The site area is 1.78 ha south of the North Middlesex University Hospital, and surrounded by nearby employment and residential properties.
- 4.3. The site has good transport links to the A406 North Circular to the north, the A10 heading north to the M25 and south into central London. Silver Street railway station is 10 minutes walking distance with services direct into Liverpool Street Station.

- 4.4. On the same block as the development site, the existing buildings to the north and north east are two- and three-storey post war brick industrial premises. These buildings are not in the applicant's control and no development of these buildings is proposed.
- 4.5. To the east of the application site, on Commercial Road, are narrow single-storey industrial buildings with residential gardens backing on to them from the east. To the south, Shaftsbury Road, is developed with two-storey industrial buildings. The most proximate residential buildings are to the west of the site, across Bull Lane and on streets off of Bull Lane. As noted, North Middlesex University Hospital is immediately to the north of Bridport Road.
- 4.6. The site is presently unoccupied. The existing buildings on site are interlinked and are generally of steel frame and cladding dating from the 1980's. Several of the ancillary buildings within the yards are of masonry construction. The existing buildings are in poor condition and inefficient. The construction and condition of the current buildings makes them challenging to reuse. The site has the following constraints: contaminated land, Locally Significant Industrial Site, Sub Station Zone and surface water flood risk.

5. Proposal

- 5.1. The proposed development is for the erection of new commercial development comprising three buildings subdivided into seven units with primary access from Commercial Road to the east (serving units 5-7), and a single retained access from Bull Lane to the north west (serving units 1-4). The access from Bull Lane must be retained as it also serves the existing units immediately abutting the proposal site to the north which are outside of the applicant's control.
- 5.2. The three buildings are of slightly varying heights with the tallest building, Unit 5, rising to a 14.5 metre parapet height and 15.1 metre ridge height. Unit 5 is over two levels and includes a double height warehouse space and office on the first floor that wraps Bull Lane and Shaftsbury Road. Units 1 through 4 at the north of the site are the smallest units, arranged over two storeys – also with a double height warehouse/production area and office on mezzanine-type first floors. Units 6 and 7 are at the south of the site and, similarly to other units, are configured with double height warehouse/production spaces and offices on the first floor.
- 5.3. Vehicular accommodation is crucial to the operation of the site. 47 car parking spaces and 72 bicycle parking spaces have been apportioned on the site with pedestrian circulation to assist in making navigation through the site safe. Bays for articulated vehicles have been located away from the concentration of car and cycle spaces. The site is further proposed to be landscaped and greened to include trees, shrubs, hedging, planted gabions, green walls and green roofs. The development incorporates SuDS measures to mitigate ground water flooding.

6. Environmental Review

- 6.1. The proposed development, when assessed in accordance with the Town and Country Planning (Environmental Impact Assessment) Regulations 2017 and

National Planning Practice Guidance, does not constitute Schedule 2 development. It is not therefore 'EIA development' and no scoping exercise or any Environmental Statement was required. The Council issued screening opinion reference 23/00454/SO.

- 6.2. On the basis of the assessment undertaken, the proposed development would not give rise to significant effects on the environment such that it would trigger the requirement for an Environmental Impact Assessment having regard to the above Regulations, National Planning Practice Guidance and relevant case law.

7. Consultations

Pre-Application Consultation

- 7.1. The applicant engaged Local Planning Authority officers in a pre-application review. In a letter dated 25 January 2022, officers set out the following comments in response to the pre-application submission.
- The site should be maximised with an uplift in floorspace, providing a range of unit sizes
 - The Bull Lane frontage should be made active, ideally with small units located along this frontage
 - The site plan and access should address changes to cycleways, modal filters and the bus gateway of the North Middlesex Hospital Active Travel Improvements project.
 - Soft landscaping of site frontages is welcomed
 - A Flood Risk Assessment and SuDS strategy should be submitted in light of surface water flood risk.
 - The proposal needs to comply with principles of sustainable development, including urban greening and Biodiversity Net Gain.
 - Environmental factors including noise, air and dust will need to be assessed and mitigated.
 - S106 obligations will include an employment skills strategy and any highways mitigation.

Statutory and Non-Statutory Consultees

- 7.2. SuDS Highways: SuDS officers have reviewed the drainage strategy and do not object to the proposal. Officers have requested a condition for a final SuDS strategy and SuDS verification report.
- 7.3. Transportation: Transportation officers have no objections. In review of the Transportation Assessment, officers undertook trip forecasting estimates and found that, while some discrepancies are likely given the nature of estimating the trip levels, officers accept the applicant's figures and traffic generation will not be to a level that would have a significant impact on traffic flow or highway safety.
- 7.4. Trees: The tree officer raises no objection to the application and recommends a condition for Landscape Maintenance and Management Strategy.
- 7.5. Environmental Health: The Environmental Health officer does not object to the application for planning permission and finds there is no significant adverse

impact that cannot be addressed through mitigation measures that have been conditioned. Conditions are recommended to require a land contamination remediation scheme, a final noise impact assessment and dust and emissions monitoring during construction.

- 7.6. Metropolitan Police (Secured by Design): The applicant engaged directly with the Secured by Design office and a condition is recommended that this applicant seeks a Secured by Design certification.
- 7.7. Thames Water Authority: Thames Water did not comment.
- 7.8. Environment Agency: The Environment Agency did not comment.
- 7.9. Natural England: Natural England did not comment.
- 7.10. London Fire Brigade: The London Fire Brigade did not comment.
- 7.11. Historic England: Historic England, in its role as a heritage authority, notified that it would not be offering advice on this application.
- 7.12. London Borough of Haringey: The London Borough of Haringey responded that it does not object to the proposal.

Public Consultation

- 7.13. Public consultation as a result of this planning application involved notification letters being sent to 389 neighbouring properties on 12 December 2022, a press advert in the Enfield Independent was published 07 December 2022 and 2 site notices were erected 7 December 2022.
- 7.14. As a result of public consultation, 1 representation was received objecting to the proposal. A summary of reasons for comment is below:
 - Traffic Generation & Congestion:
 - The existing site use as a skip hire is linked to the adjoining waste disposal site, and removal of the skip hire location will not reduce traffic by a significant volume, as the skips will still be dropped off to the waste site, and may also be collected from the waste site should they be stored there.
 - Trip estimates are not accurate due to the comparison sites selected from the TRICS database not accurately reflecting the nature of the use of the proposed site occupants, and are likely to be significantly higher than suggested.
 - No existing ATC (Automatic Traffic Counts) were undertaken, which would have given an accurate baseline level of traffic levels.

Officer response: In house review of the trip generation was undertaken, to add robustness to the estimated figures. Larger logistics sites were looked at to give a “worst case” scenario, but this was also balanced with the level of parking being provided for the units (both HGV and staff parking), and the existing use class of the site. Officers found that, while some discrepancies are likely given the nature of estimating the trip levels, officers accept the applicant’s figures and traffic

generation will not be to a level that would have a significant impact on traffic flow or highway safety.

- Highway Safety
 - Concerns with the Traffic Order consultation process (including modal filter proposals), and the potential increase in traffic accessing Commercial Road and Bull Lane.

Officer response: This objection was reviewed and responded to by Richard Eason (Journeys and Places Programme Director). These interventions are not related to the planning application.

- Access
 - Concerns that the existing access route along Bridport Road and then Commercial Road does not have the appropriate carriageway width available to accommodate large vehicles, including ones turning into the site access.

Officer response: The access is an established access. Revised swept path plans were provided to illustrate manoeuvring by larger vehicles accessing / egressing the site. Officers are satisfied that the applicant made improvements to the site layout and there is sufficient space.

8. Relevant Policy

National Planning Policy Framework 2023

- 8.1. The National Planning Policy Framework (NPPF) introduces a presumption in favour of sustainable development. In this respect, sustainable development is identified as having three dimensions - an economic role, a social role and an environmental role. For decision taking, this presumption in favour of sustainable development means:
- a) an economic objective – to help build a strong, responsive and competitive economy, by ensuring that sufficient land of the right types is available in the right places and at the right time to support growth, innovation and improved productivity; and by identifying and coordinating the provision of infrastructure;
 - b) a social objective – to support strong, vibrant and healthy communities, by ensuring that a sufficient number and range of homes can be provided to meet the needs of present and future generations; and by fostering a well-designed and safe built environment, with accessible services and open spaces that reflect current and future needs and support communities' health, social and cultural well-being; and
 - c) an environmental objective – to contribute to protecting and enhancing our natural, built and historic environment; including making effective use of land, helping to improve biodiversity, using natural resources prudently, minimising waste and pollution, and mitigating and adapting to climate change, including moving to a low carbon economy.

- 8.2. The NPPF recognises that planning law requires that applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise. The NPPF does not change the statutory status of the development plan as the starting point for decision making.
- 8.3. Paragraph 48 of the NPPF details when weight may be given to relevant emerging plans. This guidance states that the stage of preparation, the extent to which there are unresolved objections and the degree of consistency of relevant policies to the Framework are relevant.
- 8.4. The NPPF sets out at Para 11 a presumption in favour of sustainable development. For decision taking this means:

“(c) approving development proposals that accord with an up-to-date development plan without delay; or

(d) where there are no relevant development plan policies, or the policies which are most important for determining the application are out-of-date (8), granting permission unless:

(i) the application of policies in this Framework that protect areas or assets of particular importance provides a clear reason for refusing the development proposed); or

(ii) any adverse impacts of so doing would significantly and demonstrably outweigh the benefits, when assessed against the policies in the Framework taken as a whole.”

The London Plan 2021

- 8.5. The London Plan is the overall strategic plan for London setting out an integrated economic, environmental, transport and social framework for the development of London for the next 20-25 years. The following policies of the London Plan are considered particularly relevant:

GG2	Making the best use of land
GG3	Creating a healthy city
GG5	Growing a good economy
GG6	Increasing efficiency and resilience
D1	London’s form, character and capacity for growth
D2	Infrastructure requirements for sustainable densities
D3	Optimising site capacity through the design-led approach
D4	Delivering good design
D5	Inclusive design
D8	Public realm
D11	Safety, security and resilience to emergency
D12	Fire safety
D13	Agent of change
D14	Noise
E2	Providing suitable business space
E3	Affordable workspace
E4	Land for industry, logistics and services to support London’s economic function
E6	Locally Significant Industrial Sites

E7	Industrial intensification, co-location and substitution
E11	Skills and opportunities for all
G1	Green infrastructure
G5	Urban greening
G6	Biodiversity and access to nature
G7	Trees and woodlands
SI 1	Improving air quality
SI 2	Minimising greenhouse gas emissions
SI 3	Energy infrastructure
SI 4	Managing heat risk
SI 5	Water infrastructure
SI 7	Reducing waste and supporting the circular economy
SI 8	Waste capacity and net waste self-sufficiency
SI12	Flood risk management
SI 13	Sustainable drainage
T2	Healthy Streets
T3	Transport capacity, connectivity and safeguarding
T4	Assessing and mitigating transport impacts
T5	Cycling
T6	Car parking
T6.2	Office Parking
T6.5	Non-residential disabled persons parking
T7	Deliveries, servicing and construction

Mayoral Supplementary Guidance

- 8.6. Accessible London: Achieving an Inclusive Environment (October 2014)
The strategy sets out to provide detailed advice and guidance on the policies in the London Plan in relation to achieving an inclusive environment.
- 8.7. Planning for Equality and Diversity in London (October 2007)
Explores how key spatial planning issues can impact upon equality and diversity.
- 8.8. Character and Context (June 2014)
Sets out a process for acquiring knowledge of character and context, so that change is brought about in a way which is responsive to individual places and locations.
- 8.9. Sustainable Transport, Walking and Cycling (November 2022)
Helps support planning authorities and applicants in meeting the requirements of London Plan Policy T3, as well as also supporting delivery against other policies including T1 Strategic approach to transport, and T2 Healthy Streets. It provides guidance to explain how Development Plans and development proposals should support walking, cycling and public transport.
- 8.10. The Control of Dust and Emissions during Construction and Demolition (July 2014)
The aim of this supplementary planning guidance (SPG) is to reduce emissions of dust, PM10 and PM2.5 from construction and demolition activities in London.

Local Plan – Core Strategy

- 8.11. The Core Strategy was adopted in November 2010 and sets out a spatial planning framework for the development of the Borough through to 2025. The

document provides the broad strategy for the scale and distribution of development and supporting infrastructure, with the intention of guiding patterns of development and ensuring development within the Borough is sustainable. The following is considered particularly relevant:

CP13	Promoting Economic Prosperity
CP15	Locally Significant Industrial Sites
CP16	Taking Part in Economic Success and Improving Skills
CP20	Sustainable Energy Use and Energy Infrastructure
CP21	Delivering Sustainable Water Supply, Drainage and Sewerage Infrastructure
CP22	Delivering Sustainable Waste Management
CP24	The Road Network
CP25	Pedestrians and cyclists
CO 26	Public transport
CP28	Managing flood risk through development
CP30	Maintaining and improving the quality of the built and open environment
CP32	Pollution
CP36	Biodiversity
CP46	Infrastructure contributions

Local Plan – Development Management Document

- 8.12. The Council's Development Management Document (DMD) provides further detail and standard based policies by which planning applications should be determined. Policies in the DMD support the delivery of the Core Strategy. The following local plan Development Management Document policies are considered particularly relevant:

DMD20	Locally Significant Industrial Sites
DMD21	Complementary and Supporting Uses within SIL and LSIS
DMD23	New Employment Development
DMD24	Small businesses
DMD37	Achieving high quality and design-led development
DMD38	Design Process
DMD39	The Design of Business Premises
DMD45	Parking standards and layout
DMD46	Vehicle Crossovers and Dropped Kerbs
DMD47	Access, new roads and servicing
DMD48	Transport Assessments
DMD49	Sustainable Design and Construction Statements
DMD50	Environmental Assessment Methods
DMD51	Energy efficiency standards
DMD53	Low and Zero Carbon Technology
DMD54	Allowable Solutions
DMD55	Use of Roof Space/Vertical Surfaces
DMD56	Heating and cooling
DMD57	Responsible Sourcing of Materials, Waste Minimisation and Green Procurement
DMD58	Water efficiency
DMD59	Avoiding and reducing flood risk
DMD60	Assessing flood risk
DMD61	Managing surface water
DMD62	Flood Control and Mitigation Measures
DMD63	Protection and Improvement of Watercourses and Flood Defences

DMD64	Pollution Control and Assessment
DMD65	Air Quality
DMD66	Land Contamination and Instability
DMD 68	Noise
DMD69	Light Pollution
DMD79	Ecological Enhancements
DMD80	Trees on Development Sites
DMD81	Landscaping

8.13. Other Material Considerations

Enfield's Blue and Green Strategy (2021-2031)
 Enfield Climate Action Plan (2020)
 Enfield Biodiversity Action Plan
 Enfield S106 SPD (2016)
 TfL London Cycle Design Standards (2014)
 Healthy Streets for London (2017)
 Manual for Streets 1 & 2, Inclusive Mobility (2005)
 National Planning Practice Guidance
 National Design Guide (2019)

8.14. Enfield Draft New Local Plan and Draft Proposals Map

8.15. The Reg 18 document sets out the Council's preferred policy approach together with draft development proposals for several sites.

8.16. As the emerging Local Plan progresses through the plan-making process, the draft policies within it will gain increasing weight, but at this stage it has relatively little weight in the decision-making process.

8.17. The Local Plan remains the statutory development plan for Enfield until such stage as the replacement plan is adopted and as such applications should continue to be determined in accordance with the Local Plan, while noting that account needs to be taken of emerging policies and draft site proposals.

8.18. Key local emerging policies from the plan are listed below:

- SE1 Responding to the climate emergency
- SE2 Sustainable design and construction
- SE3 Whole-life carbon and circular economy
- SE4 Reducing energy demand
- SE5 Greenhouse gas emissions and low carbon energy supply

- SE6 Renewable energy development

- SE7 Climate change adaptation and managing heat risk
- SE9 Protection and improvement of watercourses
- SE10 Sustainable drainage systems
- BG2 Protecting nature conservation sites
- BG3 Biodiversity net gain, rewilding and offsetting
- BG7 Watercourses
- BG8 Urban greening and biophilic principles
- BG11 Blue and green infrastructure plans
- DE1 Delivering a well-designed, high quality and resilient Environment

DE2: Design process and Design Review Panel
D3: Inclusive design
DE8: Design of business premises
E1: Employment and growth
E3: Protecting employment locations and managing change
E4: Supporting offices
E5: Transforming Strategic Industrial Locations and Locally Significant
Industrial
Sites
E7: Providing for workspaces
E8: Local jobs, skills and local procurement
T1: Promoting sustainable transport
ENV1: Local environmental protection
D1: Securing contributions to mitigate the impact of development

ANALYSIS

9. Main Planning Issues

9.1. The main planning issues raised by the Proposed Development are:

- Principle of Development
- Socio-economic considerations
- Character and Design
- Traffic, Access and Parking
- Neighbouring Residential Amenity
- Flood Risk and Drainage
- Biodiversity, Trees and Landscaping
- Sustainable Design and Construction
- Waste
- Fire Safety
- Environmental Health
- Health Impact Assessment

10. Principle of Development

10.1. The site is within a Locally Significant Industrial Site (LSIS). Development Management Document Policy DMD 20 resists the loss of industrial uses within an LSIS. A change of use is permitted only in certain conditions. London Plan Policy E6 prioritises these sites for Use Classes B1c (now class E(g)(ii) and/or (iii)), B2 and B8. Policy E7 encourages the intensification of these uses through, among other means, more efficient use of land through higher plot ratios having regard to operational requirements and minimising impacts to the transport network.

10.2. The present application does not propose a change of use. The redeveloped site is proposed to increase the amount of industrial use through a more efficient layout that is more appropriate to its setting. The site presently includes two buildings of 12,978 sqm GIA. Three buildings with a total of seven units are proposed to include a total of 13,323 sqm GIA – an uplift of 345 sqm GIA. The proposed use classes comprise:

- E(g) (iii): any industrial process, (which can be carried out in any residential area without causing detriment to the amenity of the area)
- B2: Industrial uses other than those falling in Class E(g)
- B8: Storage and distribution

Proposed floorspace

Units 1-4	3,037 sqm (GIA)
Unit 5	7,051 sqm (GIA)
Unit 6-7	3,235 sqm (GIA)
Total	13,323 sqm (GIA)

10.3. Whilst the LPA is currently in the final stages of a new Employment Land Review, the data underpinning the extant 2018 Employment Land Review indicates a need of 251,505 sqm of industrial and logistics floorspace up to 2039.

- 10.4. On the basis that there is an evidenced need in the borough for industrial land, and policy supports intensification of existing sites, the principle of the continued use of the site for industrial purposes with an increase in floorspace is strongly supported.

11. Socio-economic considerations

- 11.1. Policy CG5 of the London Plan seeks to ensure that the benefits of economic success are shared more equally across London. Policy E11 makes clear that development should support employment, skills development, apprenticeships and other education and training opportunities in both the construction and end use phases.
- 11.2. The submitted Planning Statement estimates that the proposed development would provide floorspace for a potential 191 jobs. The Planning Statement indicates that the last known occupier employed seven people. This represents an overall net increase in jobs on the application site.
- 11.3. The Applicant would be required to use all reasonable endeavours to secure the use of Local Labour as 25% of the overall labour engaged in the demolition and construction phase, and to secure the use of Local Goods and Materials to the value of a minimum of 10% of the overall spend on construction of the Development, in accordance with the Enfield S106 SPD (2016). For a development of this scope, five on-site apprentices are to be secured. An Employment & Skills Strategy (E&SS) and monitoring of the implementation of the E&SS would be secured by a S106 obligation.

12. Character and Design

- 12.1. Paragraph 126 of the NPPF underscores the central value of good design to sustainable development. The Framework expects the planning process to facilitate “high quality, beautiful and sustainable buildings and places”. As in Paragraph 130, the assessment of a scheme should take into account the endurance of the design, visual appeal, sensitivity to local context, sense of place, optimisation of the site and contribution to health and wellbeing.
- 12.2. London Plan Policy D4 aims for proposals to go through a complete design process to encourage a high-quality outcome.
- 12.3. Enfield Policy DMD 37 sets out objectives for achieving good urban design: character; continuity and enclosure; quality of public realm; ease of movement; legibility; adaptability and durability; and diversity.
- 12.4. This proposal comes in the context of several strategic enhancements in its surroundings. There are ongoing plans with the London Borough of Haringey to upgrade the shared Bull Lane Playing Fields and facilities of the Selby Trust. In order to better tie the green space to North Middlesex University Hospital, Silver Street and the commercial centre of Angel Edmonton, improved active travel links are planned, with cycle infrastructure and better pedestrian conditions along Bull Lane and along Bridport Road. It is, therefore, important that the development on the application site reinforces these highways connections with an engaging street presence and a layout that responds well to its urban context.

Layout

- 12.5. London Plan Policy GG2 seeks to make the best use of land, including through enabling the development of brownfield land. The proposed industrial intensification within designated Local Strategic Infrastructure Land is strongly supported as it creates needed employment floor space and the opportunity for additional jobs.
- 12.6. The proposal responds well to London Plan Policy D3 to optimise site capacity through a design-led approach. The present configuration gives a significant area of the site over to parking along the Bull Lane, Shaftesbury Road and Commercial Road frontages; buildings are presently set away from the street and of a very poor quality. The proposal addresses street frontages, principally Bull Lane and Shaftesbury Road. It is positive that the design brings buildings to the street line and locates the service yard to the interior and along Commercial Road, which is heavily industrial. This approach protects pedestrians and neighbours from the car park, vehicle movements and the activity in the yard. The form of the buildings provides inherent security and removes the need for fencing along the perimeter. This is a significantly improved use of the site and relationship to the streetscape.
- 12.7. It is positive that an active frontage and entrance corner office have been provided to the south west corner of the site, diagonally from the Bull Lane Playing Fields. This gives the corner presence and improved activation.
- 12.8. The pavement is regularised along Bull Lane by the removal of redundant access points and dropped kerbs. This is welcome as it improves the level area of pavement and represents an improvement in the public realm.
- 12.9. The building is fairly legible, although entrances are generally on the interior, in anticipation of use by vehicles rather than pedestrians accessing the site from the street. As consent for signage will be sought separately via an application for advertising, the applicant is encouraged to develop an approach that improves wayfinding through the site.

Massing

- 12.10. The massing and height are generally acceptable – they are in keeping with the industrial scale of the rest of the block. It is acknowledged that the buildings are designed for industrial use. The internal arrangements, with large footprints, high ceilings and overhead lighting, are evident in the exteriors of the buildings. The Bull Lane and Shaftesbury Road junction is well designed to animate this corner and mark the relationship with the Bull Lane Playing Fields. Windows along Bull Lane and Shaftesbury Road give relief to the significant frontages.
- 12.11. Officers encouraged the applicant to activate the Bull Lane frontage to the degree possible, ideally with entrances and articulation of the façade. During the course of the application, the applicant made improvements to Bull Lane, introducing windows, the application of timber cladding, tree pits within the site frontage and stone gabions with planting. In summary, while the Bull Lane frontage doesn't include any direct means of activation with entrances, the measures that have been taken upgrade the present condition and provide a visually attractive frontage. A Landscape Management and Monitoring Plan by condition is recommended to ensure greenery is well presented and enhances the street.

Landscaping and public realm

- 12.12. The applicant has refined the landscape strategy over the course of the application and the present Landscape General Arrangement Plans demonstrate a cohesive and practical softening of the site, which is in keeping with the proposed operation. The landscape strategy incorporates trees throughout the site in locations that are adequately sheltered for good growing conditions, planted gabions, low rise hedge to define the boundary, some vertical greening on Shaftsbury Road and areas of soft landscaping on the interior of the site. The landscaping arrangement is generally supported and a Landscape Management and Monitoring Plan should be secured by condition.

Detailing and materials

- 12.13. The proposed palette of materials with timber and metal cladding are suitable for a modern industrial facility. The arrangement of materials helps to break up facades to better respond to the grain of the diverse urban context.

Inclusive Design

- 12.14. London Plan Policy D5 requires all new development to achieve the highest standard of accessible and inclusive design and to be able to be used easily and with dignity by all. All entrances would be at grade with level thresholds as shown on the submitted drawings. All the disabled parking spaces are located close to the entrances. Interior layouts allow access for wheelchair users.

Secured by Design

- 12.15. The Metropolitan Police Designing Out Crime Office met with the applicant to review the proposal, working through particular questions concerning lighting and access control. The officer recommended a condition that, prior to occupation, the project achieves a compliance with Secure by Design Guides or Crime Prevention Standards for review by the LPA and Metropolitan Police. Such a condition is recommended.

13. Traffic, Access and Parking

- 13.1. Access onto the site will be reduced from four areas of entry, which comprise two lengths of dropped kerbs on Bull Lane, one on Shaftsbury Road and one on Commercial Road to two areas of vehicular access: a bellmouth entrance on Commercial Road and one remaining area of access on Bull Lane. The Commercial Road entrance will provide access into the main yard for vehicles servicing units 5, 6 and 7. Swept path analysis demonstrates that articulated vehicles of up to 16.5 metres in length will be able to manoeuvre within the site and onto Commercial Road. The location of larger vehicles on Commercial Road appropriately concentrates movements away from Bull Lane, where active travel improvements are programmed. The remaining access from Bull Lane will serve the smaller units 1 to 4 at the north of the site. This distribution of site access is acceptable.
- 13.2. Transport officers reviewed trip generation figures provided by the applicant. Officers prepared an in-house assessment to confirm the figures. While the trip estimates prepared by officers were slightly higher than the Transportation Assessment, some discrepancies are likely given the nature of estimating the trip

levels. Officers accept the applicant's figures and concluded that the site is not likely to generate a level of traffic that prejudices highway safety or the operation of neighbouring industrial units. In order to moderate the potential for more vehicle trips than assessed for this application, a condition is recommended limiting the numbers of vehicle and HGV parking bays to the number shown in the site plan and restricting parking within the site to just the provided bays at any given time.

- 13.3. Pedestrians and cyclists enter the site via the same two vehicular entrances discussed in preceding paragraphs. Pedestrian circulation relies on entrances to buildings on the interior of the site. As noted in the urban design consideration, access from the street would be preferable, but the impact to the efficiency of internal layouts would present challenges. Given the demands of industrial operations and mitigating efforts to provide pedestrian strips within the site, pedestrian access is acceptable. A cycle shelter is located on the southern boundary of the site, between units 5 and 6. This location is suitably sheltered from other activities.
- 13.4. There are presently approximately 40 car parking spaces on the site, although some additional areas of the site may be used for parking. 47 car parking spaces are proposed within the interior of the site to serve units 5 to 7, as well as spaces adjacent to units 1 to 4 at the northern section of the application site. The proposal for 47 spaces was arrived at using a vehicle accumulation analysis to estimate the maximum number of cars parked at any point in the day. A second exercise looked at employment densities and an assumption about the numbers driving to work. Both results just exceeded 47 spaces, confirming this quantity of provision is appropriate. The vehicle parking includes electric vehicle charging points nearest building entrances for ease of use.
- 13.5. In line with London Plan standards, the development must provide a minimum of 26 long stay and 15 short stay bicycle parking spaces. The quantity and location of bicycle parking is acceptable, however, a condition is recommended for details of cycle parking to ensure compliance with design and security requirements.
- 13.6. A Construction Management Plan will be required by condition. Further investigation of vehicular access into the Commercial Road entrance will be undertaken to ensure there is adequate manoeuvring space. The funding for this as well as an active travel contribution together totalling £54,624 will be secured via S106. The active travel contribution supports measures to encourage travel by sustainable modes are implemented.

14. Neighbouring Residential Amenity

Daylight, sunlight and overshadowing

- 14.1. The nearest residential properties to the application site are on Tiverton Road, Giffard Road and Amersham Avenue to the west of the site – across Bull Lane. These properties do not front the site as they are not directly opposite the development site. Homes along Parkstone Avenue back onto Commercial Road and are separated from the application site by a long, narrow warehouse building.
- 14.2. Policy DMD 39 (1)(f) requires business premises to employ layout, landscaping and other site features, to mitigate the potential for negative impacts on surrounding uses, including consideration of access arrangements for different

uses within the site and wider area. In general, for assessing the sunlight and daylight impact of new development on existing buildings, Building Research Establishment (BRE) criteria is adopted. The BRE Guide itself emphasises that the target numerical values for measuring daylight, sunlight and overshadowing it contains are "advisory" and may be "varied to meet the needs of the development and its location". Hence, in accordance with both local and national policies, consideration has to be given to the context of the site, the more efficient and effective use of valuable urban land and the degree of material impact on neighbours.

- 14.3. A Daylight and Sunlight to Neighbouring Buildings report has been provided with the application. The report has been prepared with reference to the Building Research Establishment (BRE) Guidelines "Site Layout Planning for Daylight and Sunlight. A Guide to Good Practice" (BR209, 2022) and local planning policy. BRE guidelines are most concerned with impacts to residential or other sensitive properties. The application site is largely bounded by industrial and warehouse properties – therefore, there are no buildings relevant to BRE's criteria to the north, east or south of the application site. The nearest residential properties are to the west.
- 14.4. The BRE Guidance sets out the rationale for testing the daylight impacts of new development through various tests. The first and most readily adopted test prescribed by the BRE Guidelines is the Vertical Sky Component assessment (VSC). This test considers the potential for daylight by calculating the angle of vertical sky at the centre of each of the windows serving the residential buildings which look towards the site. The target figure for VSC recommended by the BRE is 27%, which is considered to be a good level of daylight and the level recommended for habitable rooms with windows on principal elevations. The BRE have determined that the daylight can be reduced by approximately 20% of the original value before the loss is noticeable.
- 14.5. The second method is the No Sky Line (NSL) or Daylight Distribution (DD) method, which assesses the proportion of the room where the sky is visible, and plots the change in the No Sky Line between the existing and proposed situation. It advises that if there is a reduction of more than 20% in the area of sky visibility, daylight may be affected.
- 14.6. The availability of sunlight should also be checked on all open spaces, including parks and gardens. In general, it is recommended that at least half of the amenity areas listed above should receive at least two hours of sunlight on 21 March.
- 14.7. Of all of the properties that met BRE criteria requiring that they be assessed, only one flank window at 2 Amersham Avenue achieved a VSC below 27% - it resulted in a VSC of 21%, which in urban contexts, can still be considered acceptable. This may also be a corridor window, although the test was conservative. Otherwise, all windows met VSC and daylight distribution tests. The gardens of residential properties on Giffard Road, Amersham Avenue and Parkstone Avenue were assessed for overshadowing, and found not to experience a loss of sunlight as a result of the development.

15. Flood Risk and Drainage

- 15.1. Policy SI 13 of the London Plan requires development proposals to achieve greenfield run-off rates and ensure that surface water run-off is managed as close to its source as possible. There should also be a preference for green over grey features, in line with the specified drainage hierarchy. Policy DMD 61 states developments should seek to achieve Greenfield runoff rates for 1 in 1 year and 1 in 100 year (plus climate change) year events and must maximise the use of Suds, including at least one 'at source' SuDS measure resulting in a net improvement in water quantity or quality discharging to sewer in-line with any SuDS guidance or requirements.
- 15.2. Classed as Flood Risk 1, the site is at low risk of fluvial flooding. The existing site mainly comprises impermeable surfaces. Based on the Enfield Level 1 Strategic Flood Risk Assessment, localised areas of the site are prone to surface water flooding for the 1 in 100 year (+17% climate change allowance) due to surface water generated from site.
- 15.3. During the course of the application, the applicant made amendments to the drainage strategy to introduce areas of sedum roof, permeable paving and filter strips. The discharge rates proposed for the site are suitable.
- 15.4. In order to further improve opportunity for source control drainage, officers recommend a condition requiring a final design of the drainage strategy that includes additional source control drainage features such as raised planters to capture runoff. The condition will also require details of the proposed SuDS features and a management plan for future maintenance. A drainage verification report is also recommended to ensure compliance of the final drainage details. The requested information would be secured by way of conditions in accordance with Policies SI 12, SI 13 of the London Plan (2021), Policy CP 28 of the Enfield Core Strategy (2010) and Policies DMD 59, DMD 61 and DMD 63 of the Development Management Document (2014).

16. Biodiversity, Trees and Landscaping

Trees

- 16.1. Policy G7 of the London Plan requires existing trees of value to be retained, and any removal to be compensated by adequate replacement, based on the existing value of benefits. The Policy further sets out that planting of new trees, especially those with large canopies, should be included within development proposals.
- 16.2. At a local level, Policy DMD80 of the Development Management Document stipulates that developments should not result in any loss or harm to trees of significant biodiversity or amenity value, and where there are exceptional circumstances to support the removal of such trees or adequate replacement must be provided whilst the Draft Local Plan outlines the benefits that trees offer to people and the environment by improving air quality, reducing noise pollution, contributing to climate change adaptation and reducing the urban heat island effect. Additionally, Policy DMD 81 of the Development Management Document refers to landscaping.
- 16.3. According to the submitted Tree Survey, there are presently three areas of trees on the site:

- A line of Leyland cypress along Shaftsbury Road that has been identified as a tree group and classified Category C in the survey;
 - A group of self-sown buddleja grow on the interior of the site along Bull Lane – these are classified Category C; and
 - One Norway maple is growing at the southwestern corner of the site, classified between categories B and C.
- 16.4. All of these trees and vegetative elements are proposed to be removed in order to enable a more efficient use of the site and to plant new trees where they will have more suitable conditions for growth.
- 16.5. In replacement of the two groups and one tree, the applicant proposes eight new trees in areas on the interior of the site, adjacent to parking bays. The applicant has submitted details of tree pits that show adequate space for healthy growth. In addition to the new trees, the landscaping proposal introduces areas of sedum roof, low hedging along some boundary sections, vertical planting and gabions with planting. Officers find that the proposed landscape scheme provides a coherent design and generally uses trees and shrubs which are appropriate for the available space. A requirement for a Landscape Management and Monitoring Plan by condition is recommended.

Protected species

- 16.6. The application includes a Preliminary Ecological Appraisal and Bat Roost Assessment that concludes that the present structures, vegetations and conditions on the site are of negligible ecological importance having limited potential to support biodiversity. The site was deemed to have negligible potential for roosting, foraging and commuting bats as no potential roost features were found. No sightings of protected bird species were recorded.
- 16.7. While the site presently has little ecological value, the application proposes to install bat boxes in new structures or trees, in line with London Plan Policy G6 and DMD Policy BG3. A condition is recommended to ensure details of box locations are reviewed.
- 16.8. A condition is also recommended that demolition and vegetation clearance works are completed outside the bird nesting season. The application further proposes incorporation of bird boxes, the details of which will be secured by condition.

Biodiversity Net Gain and greening

- 16.9. A Biodiversity Impact Assessment following the Natural England Metric 3.1 methodology has been undertaken to calculate the ecological value of the pre- and post-development site.
- 16.10. The existing site is dominated by building and hardstanding. The proposals include the creation of new tree, shrub, hedge, vertical planting and sedum roof habitats.
- 16.11. The Biodiversity Impact Assessment demonstrates that the proposed development would result in a net gain of 0.59 habitat units (increase of 100%) and a net loss of 0.02 hedgerow units (a decrease of 8.33%). This exceeds the emerging statutory minimum BNG requirement of 10%.

- 16.12. Policy G5 of London Plan (2021) requires major development proposals to contribute to the greening of London by including urban greening as a fundamental element of site and building design. Urban greening can be used as a means of introducing planting where a satisfactory level of BNG improvement cannot be achieved in urban contexts. In this instance, the application exceeds the London Plan requirement for Biodiversity Net Gain and the submitted Landscape General Arrangement Plans demonstrate an integral greening strategy.
- 16.13. Detailed landscaping plans and a Landscape Management and Monitoring Plan will also be secured by condition to ensure the delivery of the BNG and the greenery and its management for a period of at least 30 years in accordance with Policy DMD 81 and Policies G5 and G6 of London Plan (2021)

17. Sustainable Design and Construction

Energy

- 17.1. Policy SI 2 of the London Plan (2021) requires major development to be net zero carbon. A minimum on-site reduction of at least 35% beyond Building Regulations (2021) is required including unregulated operational emissions. Non-residential development should achieve 15 per cent through energy efficiency measures.
- 17.2. Based on the submitted revised Energy Statement, at the 'Be lean' stage, the proposal meets 12% improvement against Part L 2013 and pledges this achieves 10% over Part L 2021. Passive design has been incorporated where layout maximises the number of rooms which receive direct sunlight and balances the percentage of glazing between North and South facing windows. Facades are designed to balance the proportion of solid wall to glazing and balances the provision of daylight and winter solar heating.
- 17.3. With regard to the 'be clean' stage, the Site is located within a Heat Network Priority Area and is in close proximity to the pipe to enable connection to the District Energy Network. London Plan Policy SI 3 (D) of the London Plan (2021) requires major development proposals within Heat Network Priority Areas to have a communal low temperature heating system.
- 17.4. Given the nature of the development, tenants for the units are not yet known. The industrial spaces are being delivered to a shell and core fit out; the limited office space is to be heated by ASHP/electric water heating. Without identified users, it is unclear what heating needs may be and the applicant would prefer to defer the cost of connection until tenant requirements are clearer.
- 17.5. The applicant has engaged with Energetik who have confirmed that a route can be reserved for pipework connection until there is a clearer need to provide a source of heating. The routes between the connection points of the buildings and the DEN pipe will be safeguarded. Details of these routes will be required by condition and future connection will be safeguarded by a S106 Agreement. In the interim, the development will be prohibited from installing gas heating.
- 17.6. At the 'Be Green' stage, the development proposes installation of 1,614 sqm of photovoltaic panels, saving over 53,000 kilograms of CO₂. A condition is recommended to monitor performance.

- 17.7. The final Energy Performance Certificate with associated Building Regulations Compliance Report and Display Energy Certificate after practical completion of the building will be secured to ensure compliance of the revised Energy Statement via a condition. Furthermore, the post-occupation energy consumption will be monitored for 5 years in accordance with the London Plan Be Seen Guidance (2021). This will be secured by a 106 obligation.
- 17.8. Subject to the above-mentioned planning conditions and S106 obligations, it is considered that the proposed development would comply with the energy hierarchy and Policies SI2 and SI3 of the London Plan (2021).
- 17.9. A condition is recommended requiring that the development achieves a BREEAM New Construction 2018 rating of no less than Excellent in order to comply with Policy DMD 50.
- 17.10. No Overheating Risk Assessment was submitted as part of the application. A condition is needed to require an overheating strategy prior to the commencement of superstructure works.

18. Waste

- 18.1. Policy DMD 57 calls for the responsible sourcing of materials, waste minimisation and green procurement. The applicant has submitted a Site Waste Management Plan that sets out the broad principles of managing demolition and construction waste. In order to meet London Plan policy SI 7 and DMD 57, a detailed Site Waste Management Plan – Construction Phase and Site Waste Management Plan – Operational Phase will be required by condition. The reports should address how the construction and operation of the site will comply with London Plan Policy SI 7, the principles of a circular economy, and the requirements of a Green Procurement and Construction Plan.

19. Fire Safety

- 19.1. Policy D12 of the London Plan states that major applications should be accompanied by a fire statement, prepared by a suitably qualified third party assessor, demonstrating how the development proposals would achieve the highest standards of fire safety, including details of construction methods and materials, means of escape, fire safety features and means of access for fire service personnel. Further to the above, Policy D5 (B5) of the London Plan seeks to ensure that developments incorporate safe and dignified emergency evacuation for all building users, with fire evacuation lifts suitable to be used to evacuate people who require level access from the buildings.
- 19.2. A Fire Statement has been submitted and illustrates how provisions have been made for the safety of occupants as well as the provision of suitable access and provisions for firefighting considering the London Plan's fire safety policy requirements and the rationale for these measures. A condition will be attached to ensure compliance of the Fire Statement.

20. Environmental Health

Air Quality

- 20.1. Policy SI 1 (Improving Air Quality) of the London Plan states that any development proposal should not lead to further deterioration of existing poor air quality and not be located or operated in a manner that would subject vulnerable people to poor air quality.
- 20.2. Following review by environmental health officers, the proposal does not raise any objection. A standard condition is recommended for compliance with control standards for construction dust and emissions. A condition will also require Construction Management Plan.

Land contamination

- 20.3. The submitted contamination report concludes that a further investigation is necessary to assess the risk from vapours as well as risk to ground water. A condition is therefore needed for a land contamination strategy including investigation and mitigating measures to be submitted for review by officers.

Noise

- 20.4. Given proximity to residences on Tiverton Road and Giffard Road, the normal requirement is that the sound from a development does not exceed a level of 5dB below the typical background sound level. A condition is recommended requiring an acoustic report detailing what measures will be taken to ensure the noise from the use of the site does not exceed a level of 5dB below the typical measured background noise level.

21. Health Impact Assessment

- 21.1. London Plan Policy GG3 outlines that to improve Londoners' health and reduce health inequalities, those involved in planning and development must adhere to an outlined criteria.
- 21.2. The HIA should be based on the Healthy Urban Development Unit (HUDU) Rapid HIA Assessment Tool.
- 21.3. The HUDU Rapid HIA Assessment Tool provides 11 determinants of health:
 - Housing design and affordability
 - Access to health and social care services and other social infrastructure
 - Access to open space and nature
 - Air quality, noise and neighbourhood amenity
 - Accessibility and active travel
 - Crime reduction and community safety
 - Access to healthy food
 - Access to work and training
 - Social cohesion and inclusive design

- Minimising the use of resources
- Climate change

21.4. The application includes a Health Impact Assessment that reviews the proposal against each of the outlined criteria. In summary, the development presents an improvement in each category from the present condition. The proposed landscaping creates a better environment within the site and in relation to the street; the site is more accessible by bicycle; it generates economic benefits with a greater employment area and spaces that are suitable to a larger selection of businesses; it makes better use of land; and is of more modern construction to meet current building requirements. Officers are satisfied that the development addresses Policy GG3's outlined criteria.

22. CIL/S106

Mayoral CIL

22.1. The Mayoral CIL is collected by the Council on behalf of the Mayor of London. The amount that is sought for the scheme is calculated on the net increase of gross internal floor area multiplied by an Outer London weighting (increased to £60per sqm as of 1st April 2019). The estimated Mayoral CIL liability is £20,700.00.

Enfield CIL

22.2. The Council introduced its own CIL on 1st April 2016. The proposed uses of Use Classes E(g)(iii), B2 and B8 have a charging rate of £0. The estimated Enfield CIL liability is £0.00.

22.3. The estimated Mayoral and Enfield CIL liabilities above have been calculated using the Exacom public CIL calculator. CIL liability may change and would be confirmed by the CIL team at the point of issuing the CIL liability Notice.

S106 Heads of Terms

22.4. The table below outlines the Heads of Terms of financial and non-financial contributions to be secured within the S106 Agreement.

Heads of Terms	Description	Sum
Transport	Active travel and mitigation contribution	£54,624
	Travel Plan	N/A
Climate Change	Be-Seen monitoring	N/A
	District Heat Network connection strategy	N/A
Employment and skills	Employment and skill strategy	N/A
S106 monitoring fees	5 per cent of the total value of all contributions; and A fixed charge to manage non-monetary obligations of £350 per head of term	

23. Public Sector Equalities Duty

23.1 In line with the Public Sector Equality Duty the council must have due regard to the need to eliminate discrimination and advance equality of opportunity, as set out in section 149 of the Equality Act 2010. Section 149 of the Act requires public

authorities to have due regard to several equality considerations when exercising their functions including decision making on planning applications. These considerations include: Eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under this Act; Advance equality of opportunity between persons who share a relevant protected characteristic (explained in detail below) and persons who do not share it; Foster good relations between persons who share a relevant protected characteristic and persons who do not share it.

- 23.2 The main objective of the duty is to ensure public policies and programmes are implemented fairly, in particular with regard to their impact on the protected characteristics identified above. In making this recommendation, due regard has been given to the Public Sector Equality Duty and the relevant protected characteristics (age, disability, gender reassignment, marriage / civil partnership, pregnancy and maternity, race, religion or belief, sex, and sexual orientation).
- 23.3 When determining the planning application (and thereby accounting for the representations resulting from public consultation), the Council has considered the potential effects of the proposed development on those with protected characteristics as defined under the Equality Act 2010. In doing this, the Council has had due regard to equality considerations and attribute appropriate weight to such considerations. In providing the recommendation to Members that planning consent should be granted, officers have considered equalities impacts in the balance, alongside the benefits arising from the proposed development. The Council has also considered appropriate mitigation to minimise the potential effects of the proposed development on those with protected characteristics.
- 23.4 There are no statutory or regulatory requirements for the form or content of an equalities assessment. The scale and significance of such impacts cannot always be quantified, and it is common to address this through descriptive analysis of impacts and identifying whether such impacts are adverse or beneficial. The key elements of the proposed development which have an impact that could result in an equalities effect include the design and physical characteristics of the proposals subject to the planning application. Officers do not consider there would be a disproportionate equalities effect.

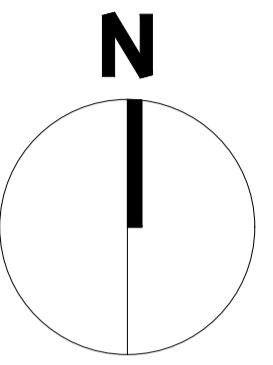
24. Conclusion

- 24.1 The proposed development brings substantial planning benefits. Officers are satisfied that, with conditions and planning obligations as set out, the development is acceptable in all areas of assessment.
- 24.2 Officers recognise the public benefits of the application:
- The provision of a modern, suitably designed and constructed industrial space in a designated Locally Significant Industrial Site.
 - Optimisation of the site by rectifying the site layout and providing a net increase of 345 sqm of floorspace.
 - The potential for an increase in employment on the site from the current condition.
 - Provision of seven industrial units of varying size and configuration, providing accommodation to a larger variety of businesses.
 - Improving the streetscape with more engaging frontages and integrated landscaping.

- Reducing vehicle access on Bull Lane to make the street safer as a strategic cycle link.
- Substantial improvement to on-site sustainable drainage.

24.3 Officers recommend that, subject to the completion of a s106 legal agreement and conditions, the application is approved.

Revisions:	Drawn/Chkd:	Date:
A. Additional door added to south elevation of Unit 5.	AMc	Dec 22
B. Bull Lane elevation & carpark revised	AH	April 23
C. South West corner amended for cycling route.	AH	May 23
D. Fence to Bull Lane omitted.	AMc	June 23
E. GIA areas updated.	AMc	Sept 23
F. Additional tree to front of Unit 1 added.	AMc	Oct 23



KEY

- SITE BOUNDARY
- EXISTING BUILDINGS TO BE DEMOLISHED
- NEW RETAINING WALLS
- 2.4m WELDMESH FENCING
- TIMBER SCREEN
- GREEN ROOF SHELTER, SHEFFIELD CYCLE HOOPS
- GREEN ROOF SHELTER, DOUBLE STACK CYCLE RACKS
- ELECTRIC VEHICLE CHARGING POINT (TWO VEHICLES PER POST)
- DUCTING SUPPLIED TO PASSIVE ELECTRIC VEHICLE CHARGING POINT
- BLOCK PAVING (VEHICULAR AREAS) CHARCOAL
- BLOCK PAVING (PEDESTRIAN AREAS) NATURAL
- BRUSHED CONCRETE SERVICE YARD
- TARMACADAM SURFACING
- WELL CONSOLIDATED GRAVEL WITH FINES
- SOFT LANDSCAPING
- PROPOSED TREES
- PROPOSED SUBSTATION AND SWITCH ROOM
- 2.4M HIGH WELDMESH FENCE WITH ACCESS GATES
- EXISTING TREES TO BE REMOVED
- AC AIR CONDITIONING PLANT

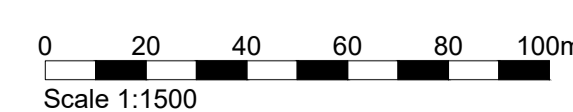


AREA SCHEDULE GROSS EXTERNAL AREA					
UNIT NO.	GF	FF	TOTAL	CAR PARKING	CYCLE PARKING
1	876m ²	244m ²	1,120m ² / 12,066ft ²	04	8
2	464m ²	127m ²	591m ² / 6,368ft ²	02	8
3	549m ²	152m ²	701m ² / 7,545ft ²	02	8
4	651m ²	181m ²	832m ² / 8,962ft ²	03	8
5	5,645m ²	1,050+640m ²	7,335m ² / 78,953ft ²	22	22
6	1,255m ²	342m ²	1,597m ² / 17,192ft ²	07	10
7	1,434m ²	388m ²	1,822m ² / 19,611ft ²	07	8
TOTAL			13,998m² / 150,673ft²	47	72

26 ELECTRIC CAR CHARGING SPACES

AREA SCHEDULE GROSS INTERNAL AREA					
UNIT NO.	GF	FF	TOTAL	NOTES	
1	829m ²	216m ²	1,045m ² / 11,258ft ²	FF 20%	CIH 8m
2	444m ²	114m ²	558m ² / 6,014ft ²	FF 20%	CIH 8m
3	526m ²	137m ²	663m ² / 7,137ft ²	FF 20%	CIH 8m
4	612m ²	161m ²	773m ² / 8,323ft ²	FF 20%	CIH 8m
5	5,475m ²	970+605m ²	7,050m ² / 75,886ft ²	FF 15%	CIH 12m
6	1,193m ²	311m ²	1,504m ² / 16,197ft ²	FF 20%	CIH 10m
7	1,368m ²	351m ²	1,719m ² / 18,503ft ²	FF 20%	CIH 10m
TOTAL			13,312m² / 143,318ft²		

SITE AREA (RED LINE) 4.42A / 1.78H
 SITE DENSITY CIRCA 69%



Client:
HE2 UK ENTERPRISES 12 GP LIMITED (LONDON)



Project:
COMMERCIAL ROAD EDMONTON

12 Warren Yard,
 Warren Park,
 Milton Keynes,
 MK12 5HW
 01908 305 246

Drawing Title:
PROPOSED SITE LAYOUT

Scale @ A1: 1:500
 Checked by: **AMc**
 Date: **OCT 22**

Job No: 11407
 Stage: PL 003
 Drawing No.: 003
 Rev: F

Issue Status:
 Construction Preliminary
 Information Approval
 Tender

Architecture
 Planning
 Master Planning
 Urban Design
 Interiors
 Landscape

Offices
 Working
 London
 Milton Keynes
 Warsaw



Proposed site entrance off Commercial Road



Proposed view looking north along Bull Lane



Proposed Unit 5 office & yard

Offices
 Woking
 London
 Milton Keynes
 Warsaw

Architecture
 Planning
 Master Planning
 Urban Design
 Interiors
 Landscape

12 Warren Yard,
 Warren Park,
 Milton Keynes,
 MK12 5NW
 01908 305 246

info@prc-group.com
 www.prc-group.com

Revisions:

Drawn/Chkd: Date:

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Client:
 HE2 UK ENTERPRISES 12 GP LIMITED (LONDON)

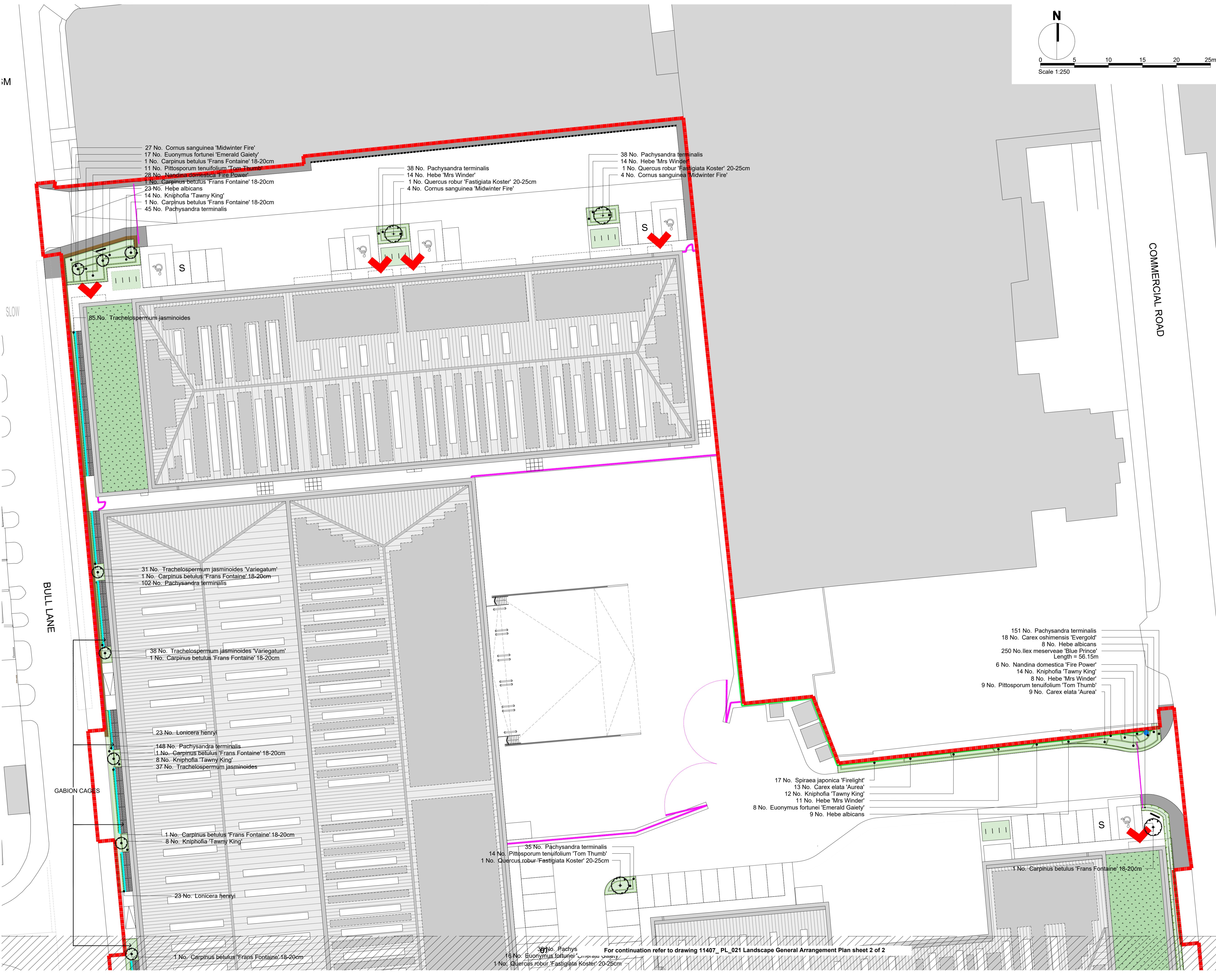
Project:
 COMMERCIAL ROAD, EDMONTON

Drawing Title:
 SITE CGI'S

Scale @ A3: NTS	Checked by: AMc	Date: OCT 22
Job No: 11407	Stage: PL	Drawing No: 007
Construction <input type="checkbox"/>	Preliminary <input type="checkbox"/>	Information <input type="checkbox"/>
Approval <input checked="" type="checkbox"/>	Tender <input type="checkbox"/>	

PRC Architecture & Planning





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Revisions:

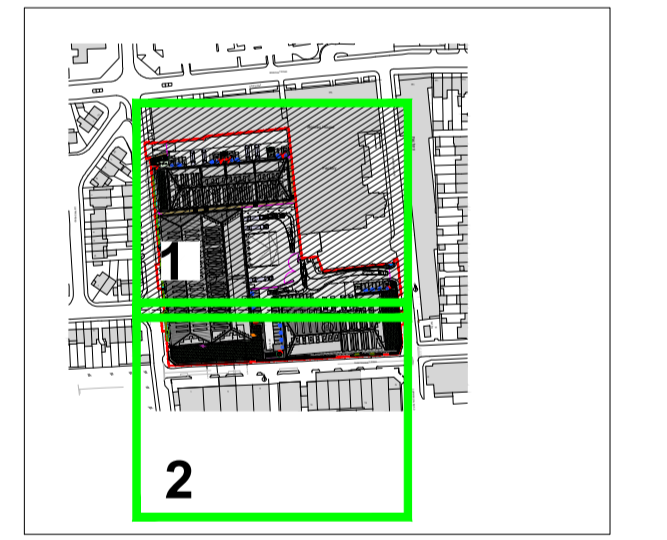
Revisions:	Drawn/Chkd:	Date:
A. The landscape plan revised accordingly to the site changes.	KC	MAY'23
B. The green vertical facade updated on the legend.	KC	JUL'23
C. The planning comments incorporated.	KC	SEP'23
D. The planting schedule updated and the note added to the legend.	KC	OCT'23

- Legend
- DETAIL PLANNING APPLICATION BOUNDARY
 - PROPOSED SPECIMEN TREE
 - PROPOSED ORNAMENTAL GROUND COVER SHRUB PLANTING
 - PROPOSED DEFENSIBLE HEDGE PLANTING - A clipped hedge, planted 450mm centers in a double staggered row. Rows 500mm apart and hedge to be maintained at 0.6 m high.
 - PROPOSED CLIMBING PLANTS Gabion cages to create structure for the climbers. To be installed as per manufacture instructions.
 - PROPOSED GREEN VERTICAL FACADE - Trachelospermum jasminoides to be grown on the proposed green walls.
 - TREE GRILLE
 - SEDUM GREEN ROOF or as equally approved.
 - SEDUM GREEN ROOF or as equally approved.

For planting schedule please refer to drawing 11407_PL_021C Landscape GA Sheet 2 of 2.

ECOLOGICAL ENHANCEMENTS - All shown indicatively

- BUG HOTEL To be installed as per the ecologists recommendations
- BAT BOXES To be installed as per the ecologists recommendations
- BIRD BOXES To be installed as per the ecologists recommendations



Location Plan NTS

Client:
HE2 UK ENTERPRISES 12 GP LIMITED (LONDON)



Project:
COMMERCIAL ROAD EDMONTON

12 Warren Yard, Warren Park, Milton Keynes, MK12 5HW
01908 305 246

Drawing Title:
LANDSCAPE GENERAL ARRANGEMENT PLAN SHEET 1 OF 2

Architecture
Planning
Master Planning
Urban Design
Interiors
Landscape

Scale @ A1:	Checked by:	Date:
1/250	AMc	OCT'22

Job No:	Stage:	Drawing No:	Rev:
11407	PL_020	D	

Issue Status:

Construction	<input type="checkbox"/> Preliminary	<input type="checkbox"/>
Information	<input type="checkbox"/> Approval	<input checked="" type="checkbox"/>
Tender	<input type="checkbox"/>	<input type="checkbox"/>

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For continuation refer to drawing 11407_PL_021 Landscape General Arrangement Plan sheet 2 of 2

Figured dimensions only are to be used. All dimensions to be checked onsite. Differences between drawings and between drawings and specification or bills of quantities to be reported to the PRC Group.

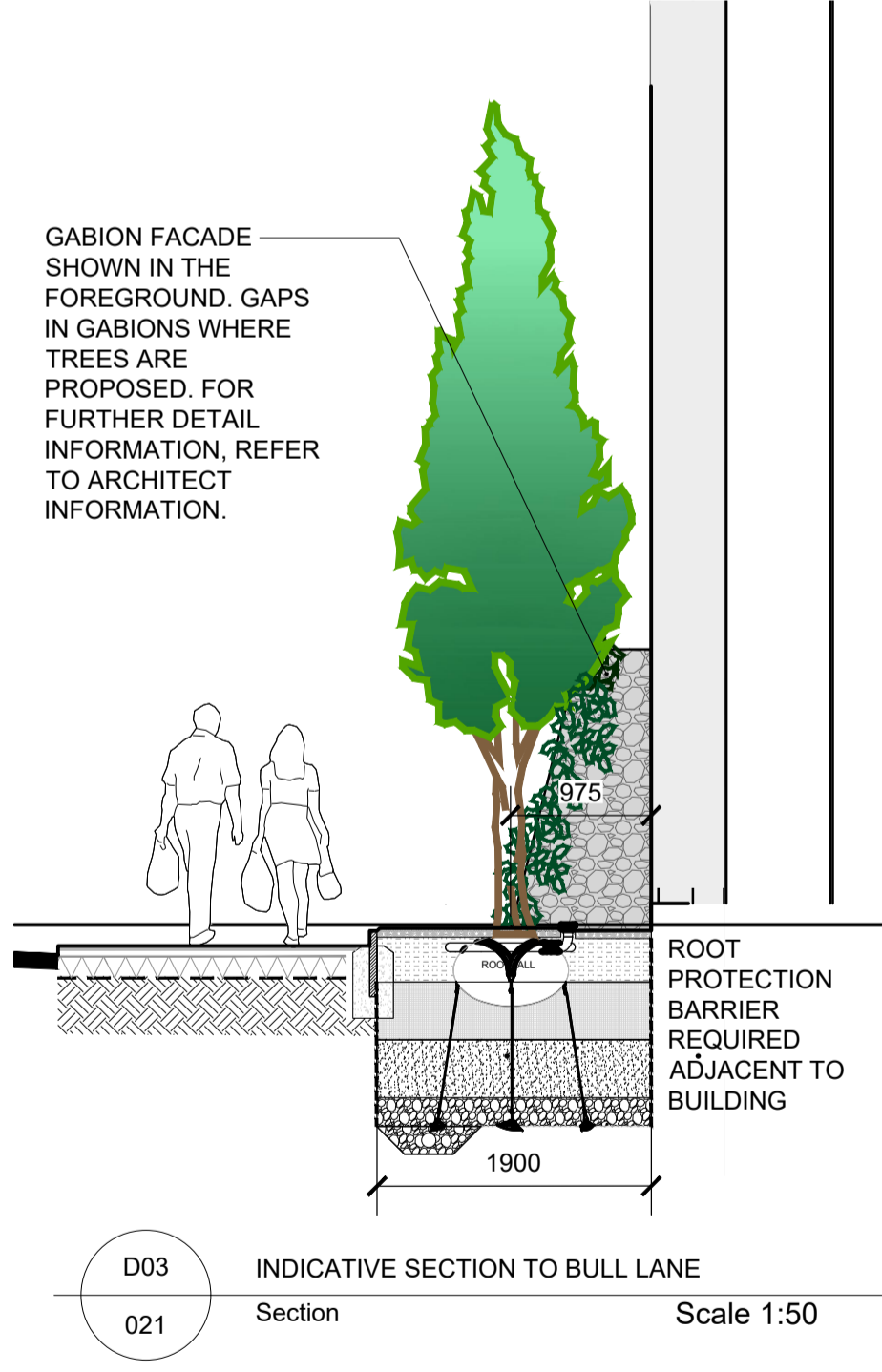
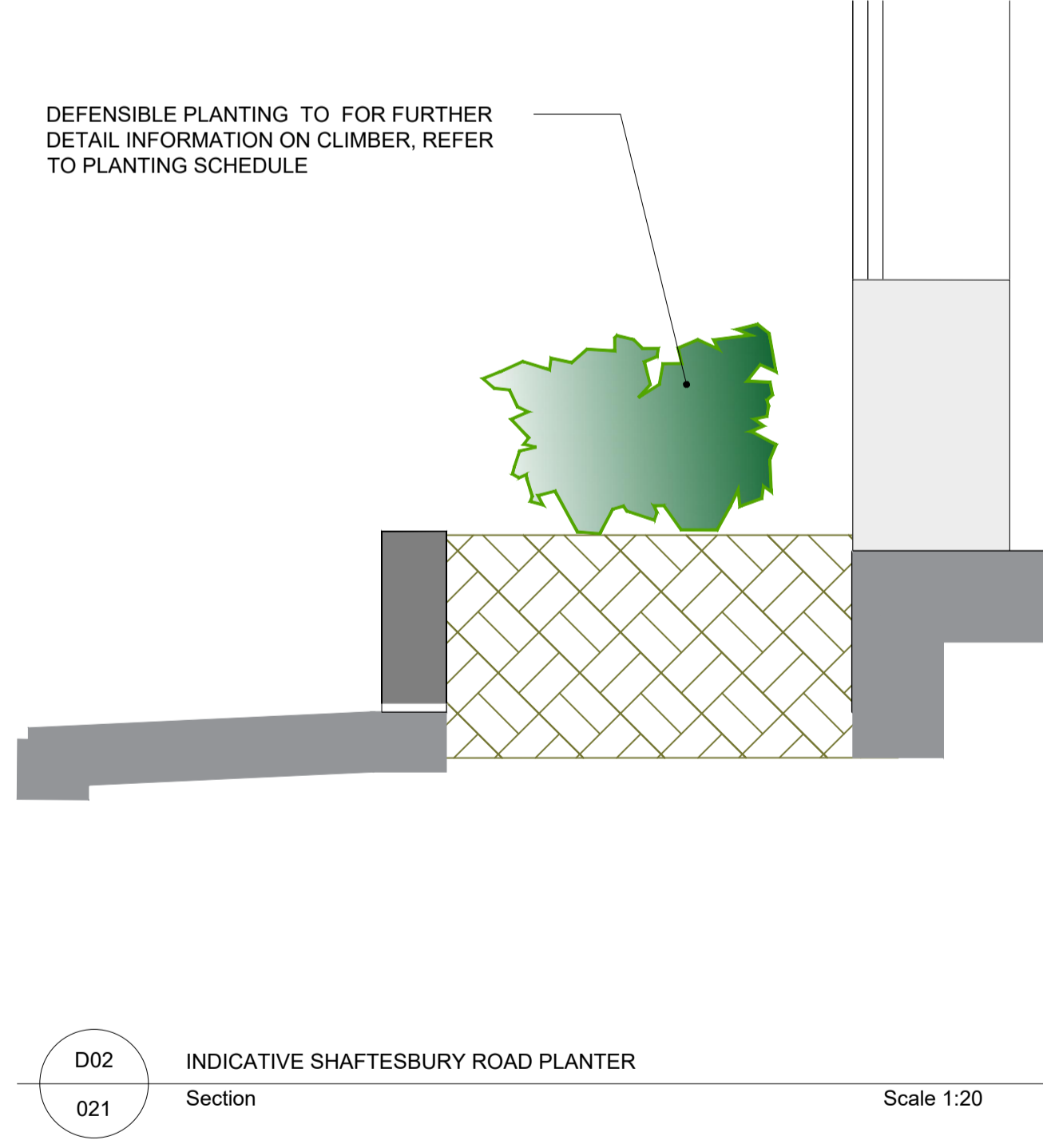
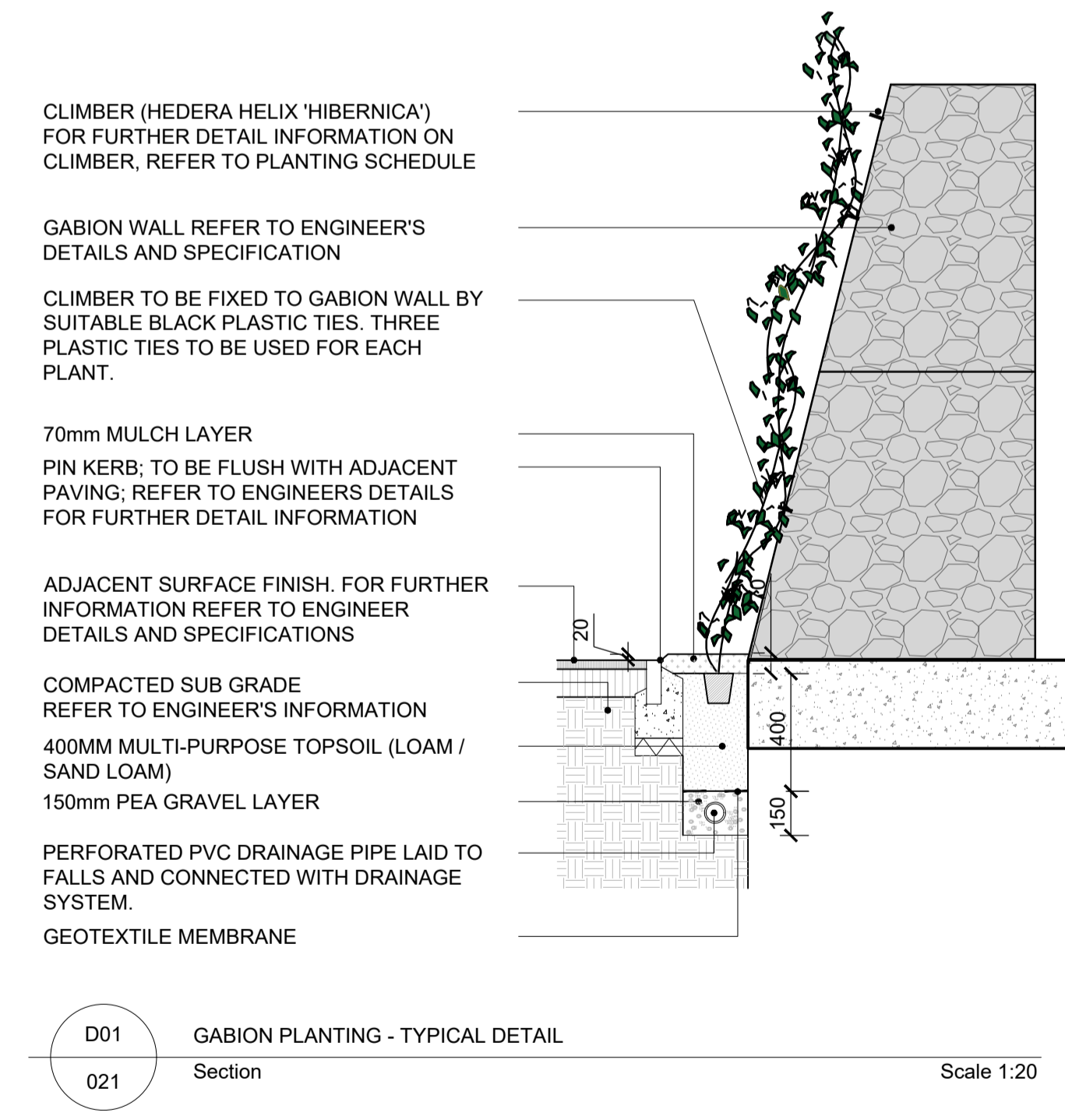
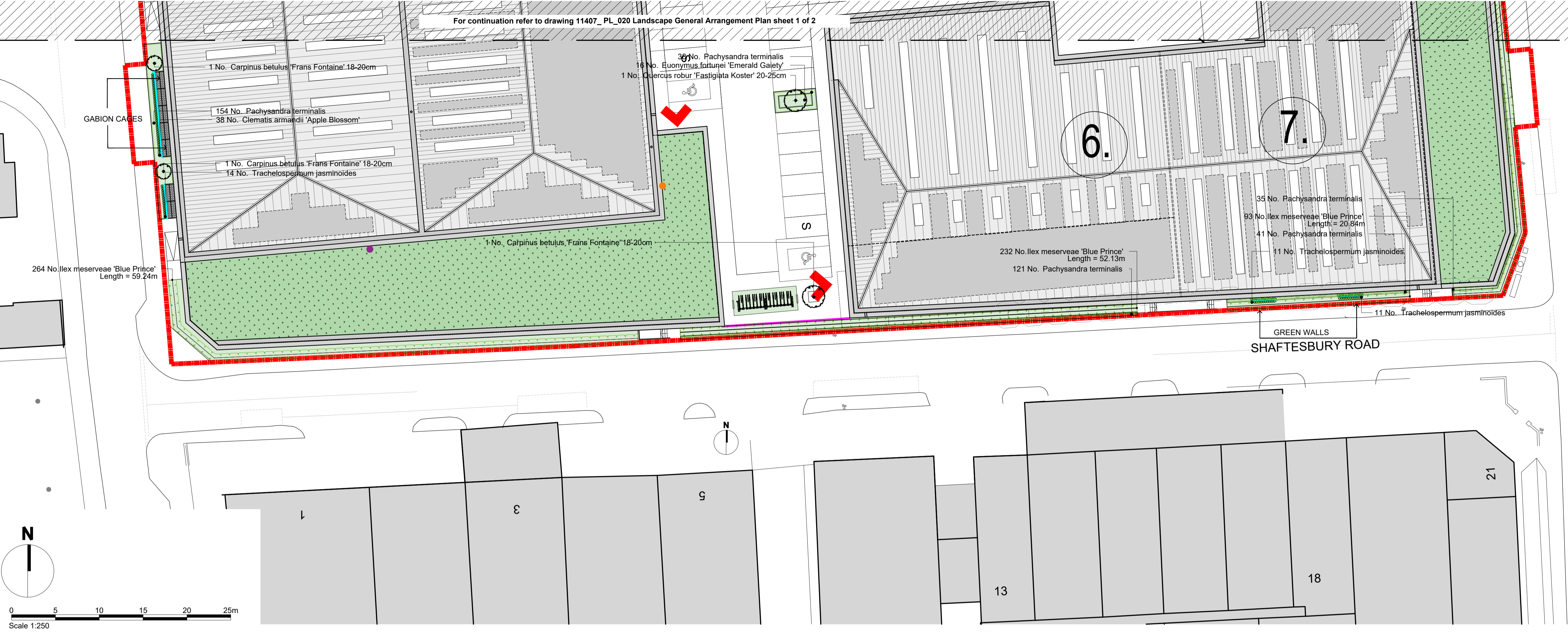
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Revisions:

Drawn/Chkd:	Date:
A. The landscape plan revised accordingly to the site changes.	KC MAY'23
B. The green vertical facade updated on the legend.	KC JUL '23
C. The planning comments incorporated.	KC SEP'23
D. The planting schedule updated and the note added to the legend.	KC OCT'23

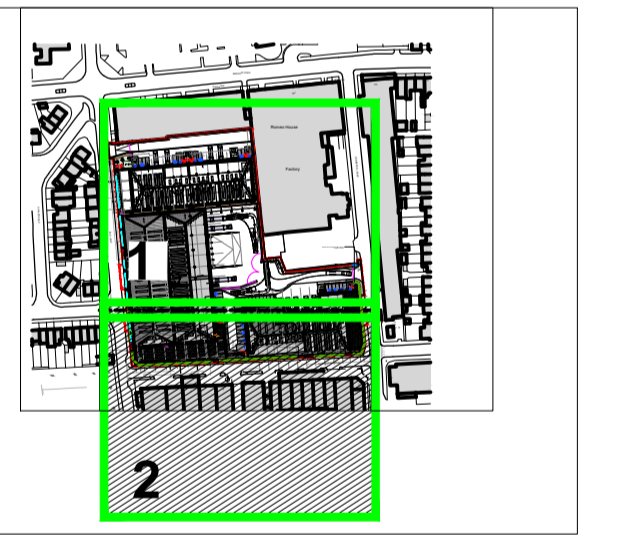
Legend

	DETAIL PLANNING APPLICATION BOUNDARY
	PROPOSED SPECIMEN TREE
	PROPOSED ORNAMENTAL GROUND COVER SHRUB PLANTING
	PROPOSED DEFENSIBLE HEDGE PLANTING - A clipped hedge, planted 450mm centers in a double staggered row. Rows 500mm apart and hedge to be maintained at 0.6 m high.
	PROPOSED CLIMBING PLANTS GABION CAGES TO CREATE STRUCTURE FOR THE CLIMBERS. TO BE INSTALLED AS PER MANUFACTURE INSTRUCTIONS.
	PROPOSED GREEN VERTICAL FACADE - Trachelospermum jasminoides, to be grown on the proposed green walls.
	TREE GRILLE
	SEDUM GREEN ROOF or as equally approved.
	SEDUM GREEN ROOF or as equally approved.
ECOLOGICAL ENHANCEMENTS - All shown indicatively	
	BUG HOTEL To be installed as per the ecologists recommendations
	BAT BOXES To be installed as per the ecologists recommendations
	BIRD BOXES To be installed as per the ecologists recommendations



Planting Schedule

Trees						
Number	Species	Specification	Height	Girth	Pot Size	
11	Carpinus betulus 'Frans Fontaine'	Extra Heavy Standard; C: Clear Stem min. 200cm	450-500cm	18-20cm	100-200L	
4	Quercus robur 'Fastigiata Koster'	Semi-Mature; C: Clear Stem min. 200cm	550-650cm	20-25cm	150-250L	
Total :15						
Shrubs						
Number	Species	Specification	Height	Density	Pot Size	
22	Carex elata 'Aurea'	Full Pot: C		3/m ²	3L	
18	Carex oshimensis 'Evergold'	Full pot		4/m ²	3L	
35	Cornus sanguinea 'Midwinter Fire'	Branched; 4 brks: C	60-80cm	2/m ²	5L	
41	Euonymus fortunei 'Emerald Gaiety'	Bushy; 7 breaks	20-30cm	3/m ²	3L	
47	Hebe 'Mrs Winder'	Bushy; 5 brks: C	30-40cm	3/m ²	3L	
40	Hebe albicans	Bushy; 5 brks: C		4/m ²	3L	
56	Kniphofia 'Tawny King'	Full Pot: C		3/m ²	3L	
34	Nandina domestica 'Fire Power'	Bushy; 4 brks: C		5/m ²	3L	
946	Pachysandra terminalis	Several shoots; 9 breaks		6/m ²	3L	
34	Pittosporum tenuifolium 'Tom Thumb'	Bushy; 3 breaks	15-20cm	3/m ²	3L	
17	Spiraea japonica 'Firelight'	Bushy; 6 brks: C	30-40cm	3/m ²	3L	
Climbers						
Number	Species	Specification	Height	Pot Size		
38	Clematis armandii 'Apple Blossom'	Caned; several shoots; 3 breaks	100-150cm	10L		
46	Lonicera henryi	Caned; several shoots; 3 breaks	175-200cm	5L		
22	Trachelospermum jasminoides	Caned; several shoots; 5 breaks	175-200cm	5L		
136	Trachelospermum jasminoides	Caned; several shoots; 5 breaks	150-175cm	5L		
69	Trachelospermum jasminoides 'Variegatum'	Caned; several shoots; 5 breaks	150-175cm	5L		
Hedge Ilex meserveae 'Blue prince'						
Number of Plants	Species	Specification	Density	Height	Pot Size	Length
839	Ilex meserveae 'Blue Prince'	Leader with Laterals: C 0.45Cr Double Staggered at 0.45m offset		30-40cm	3L	
Total :188.36m						



Location Plan NTS

Client:
HE2 UK ENTERPRISES 12 GP LIMITED (LONDON)

Project:
COMMERCIAL ROAD EDMONTON

Drawing Title:
LANDSCAPE GENERAL ARRANGEMENT PLAN SHEET 2 OF 2

Scale @ A1: AMc
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Date: D

Job No: 11407
Stage: PL_021
Drawing No: D

12 Warren Yard, Warren Park, Milton Keynes, MK12 5SW 01908 305 246
info@prc-group.com www.prc-group.com



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NOTES

- All work to be carried out to the satisfaction of the landscape architect and in accordance with the relevant current British Standards.
- No planting position shall be altered without the approval of the landscape architect and local planning authority.
- Landscape areas shall be top soiled to the following minimum depths grassed areas -300mm, shrub areas - 450mm. Standard tree pits refer to details.
- All topsoil to BS3882 general grade. Top soil areas to be cultivated by hand or machine prior to planting with minimal compaction by machines. Tree pits shall incorporate tree fertilizer to manufacturers specification.
- All areas to be soft landscaped shall be protected from compaction associated with development. In accordance with BS 5837 (part 6.2.2). If compaction has occurred soil shall be ripped to a sufficient level to break any subsoil pan before cultivation. For individual trees decompaction within restricted areas shall be achieved by through excavation of a minimum tree pit 400mm deep by 4m2 and loosening the subsoil layer for a further 300-450mm, the pit shall then be refilled with new top grade top soil.
- All standard trees to be underground guyed.
- All plant material procedures are to conform to the relevant British Standards (3936/4428) codes of practice and the specification.
- All plants shall adhere to the industry standard BS 3936-1:1992 , nursery stock specification for trees and

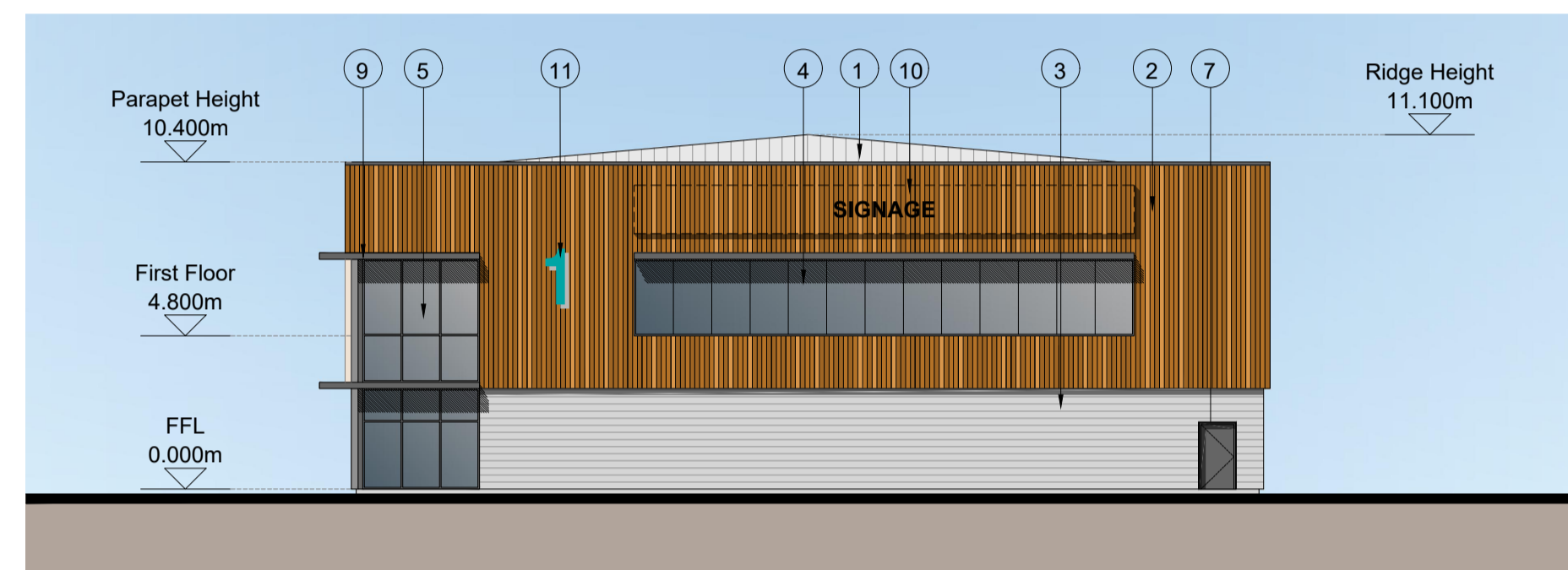
- shrubs and be in accordance with the national plant specification (NPS). All nurseries suppliers shall be registered under the HTA nursery certification scheme.
- All plants shall be packed and transported in accordance with the code of practice for plant handling as produced by CPSE. Planting shall not be carried out when the ground is waterlogged, frost bound or during periods of cold drying winds. no bare root stock has been specified but if substitution are made, all bare root stock shall be root dipped in an approved water-retained polymer. All stock to be of native provenance. container planting should be kept shaded and watered.
- All supplied container grown shrubs to be planted in holes big enough to accommodate the plant without damaging the roots (min. 300mm3). Plants to be firmed, watered in and dead/damaged branches removed after planting.
- P4 polymer shall be applied at manufacturers recommended quantities during planting.
- A slow release fertilizer shall be applied after planting in accordance with manufacturers instructions.
- Following planting supply and spread evenly Melcourt Industries "Rustic Biomulch" to a depth of 50mm, (75mm if no sheet mulch specified), over all planted areas. Finished mulch levels shall be no higher than 15mm below pavement or grass levels to avoid any spillage onto pavements or lawns. N.B. no substitution of mulch type will be acceptable. It is essential to have a no-fines, large particled, brown wood chip to reduce weed growth, reduce wind blow and prevent rapid break down.
- Before turfing, rotavate and level topsoil as required, remove debris and stones above 30mm dia. Add pre-seeding fertilizer to manufacturers specification. Turf shall be weed free, laid from planks (to avoid

- damage) with broken joints and well butted up. Finished compacted levels to be a minimum of 20mm above adjacent hard surfacing. Water as necessary to avoid shrinkage.
- Grass seed shall be sown during march/april or september. Topsoil to be rotavated and levelled as required, removing debris and stones above 320mm dia. Preseeding fertilizer to be added at manufacturers specification. seed to be applied at a minimum of 50g per metre. Area to be lightly watered and raked over after sowing where possible to aid establishment. finished compacted levels as above.
- Where appropriate hedge species shall be pruned to a reduction in height by 30% immediately upon planting.
- Tree positions have been chosen in accordance with practice note 4.2 of the N.H.B.C. guide, Foundations where required foundations will be designed to accommodate trees and shrubs.
- All preparations, both hard and soft under/around trees must be carried out by hand to protect roots.
- Planting will commence during the first planting season (mid nov-mid march) after substantial completion of the site.
- Any tree, shrub or area of turf shall be replaced within a period of five years if they die within the next planting season after planting with another such plant of similar size and species.
- Peat, or peat based material shall not be used where possible on site, where possible all plants shall be obtained in a peat free compost when container grown plants are specified.
- When planting, allow for water retention granules (eg 'broadleaf p4' or equal) for all plants at the manufacturer's suggested rate.

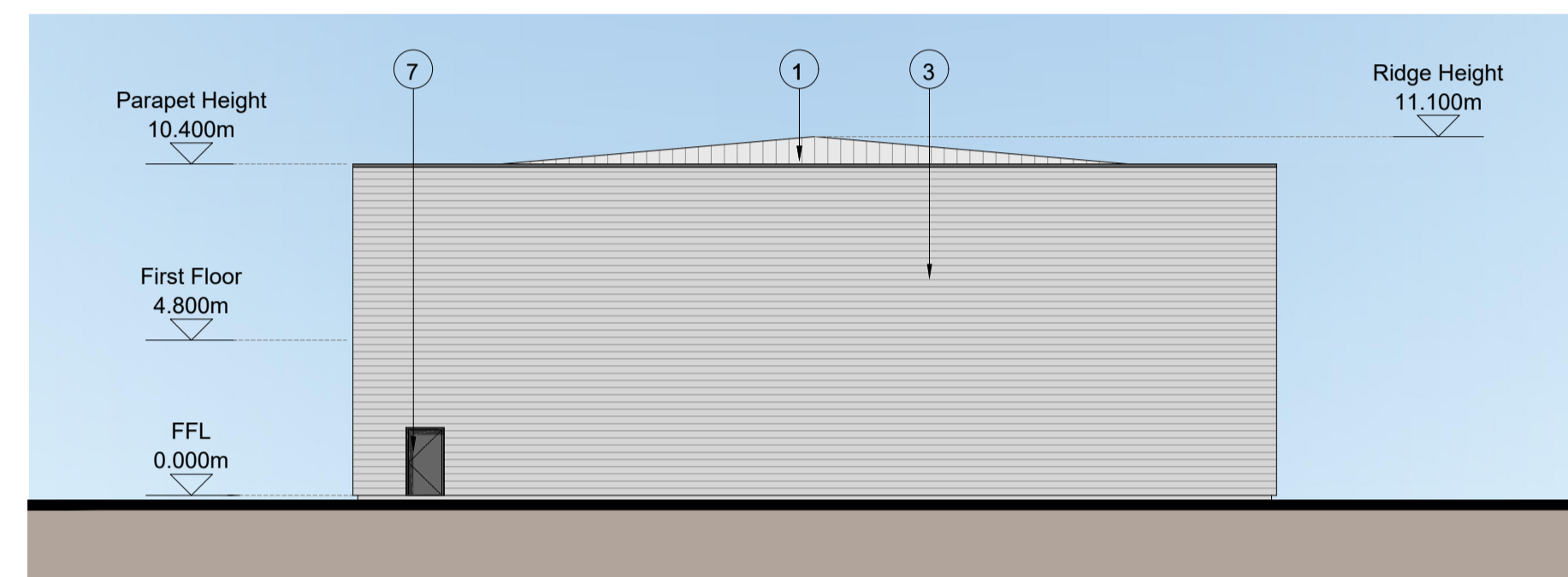
- On delivery, all bare root plants should be root-dipped (eg with broadleaf root-dip or equal) at the manufacturers suggested rate to prevent drying out and to lessen planting shock.
- All trees to be underground guyed. Allow for 1no. 80ltr bag of tree planting and mulching compost per tree to be mixed with the tree pit backfill. while establishing a 1000mm square should be kept free of grass around the tree.
- All plants will need protection from rabbits.
- If strimmers are to be used for maintenance then all standard trees in grass must be fitted with strimmer guards.
- The green wall on the units, the support to the climbers in form of the steel work which come out of the unit with a grit wire for the climbers to grow. The 0.5 m area of trench to get the climbers to grow up.



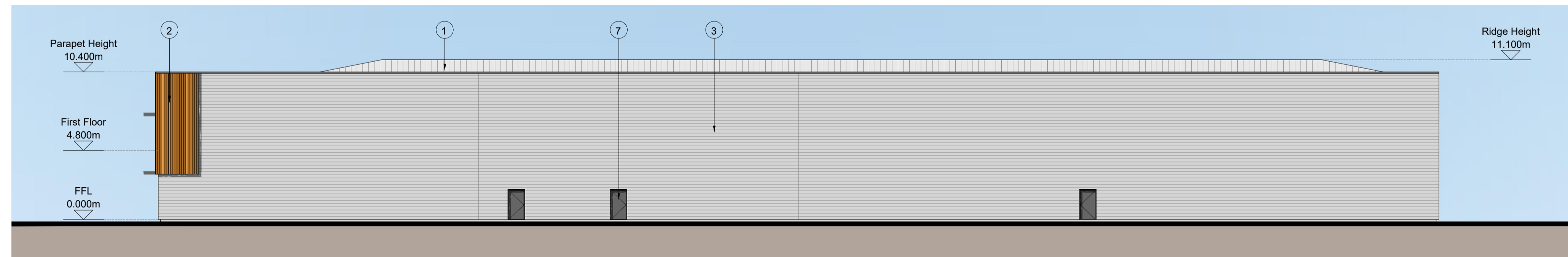
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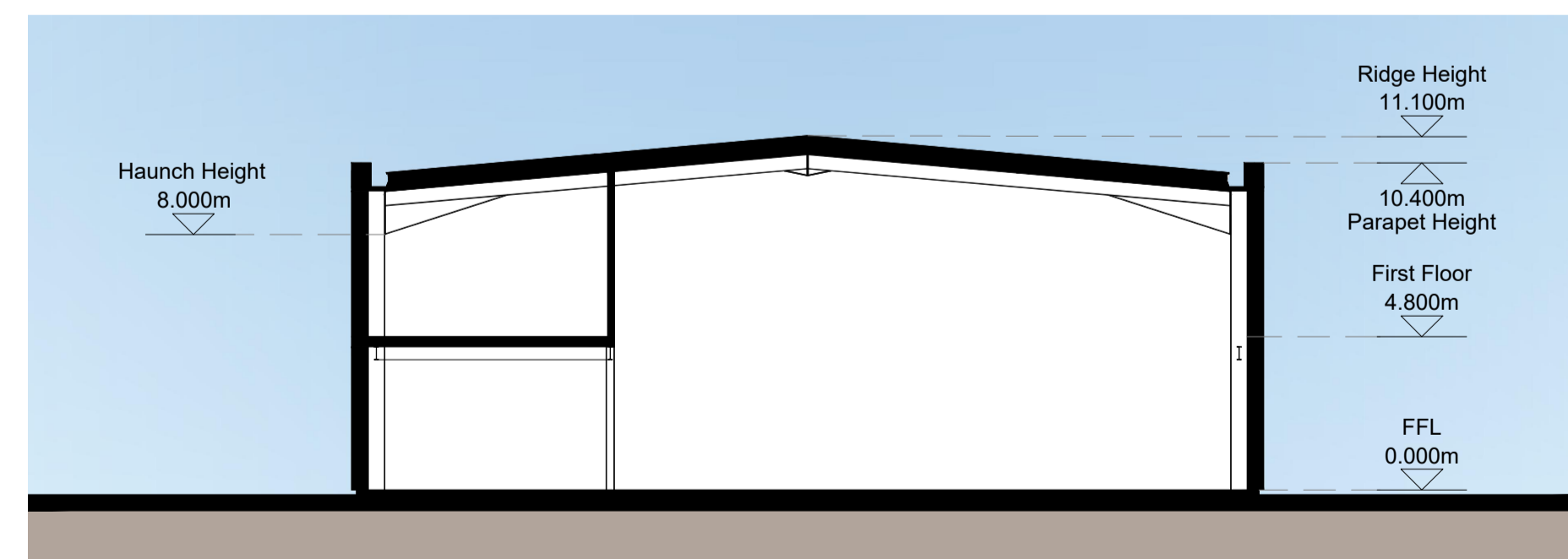
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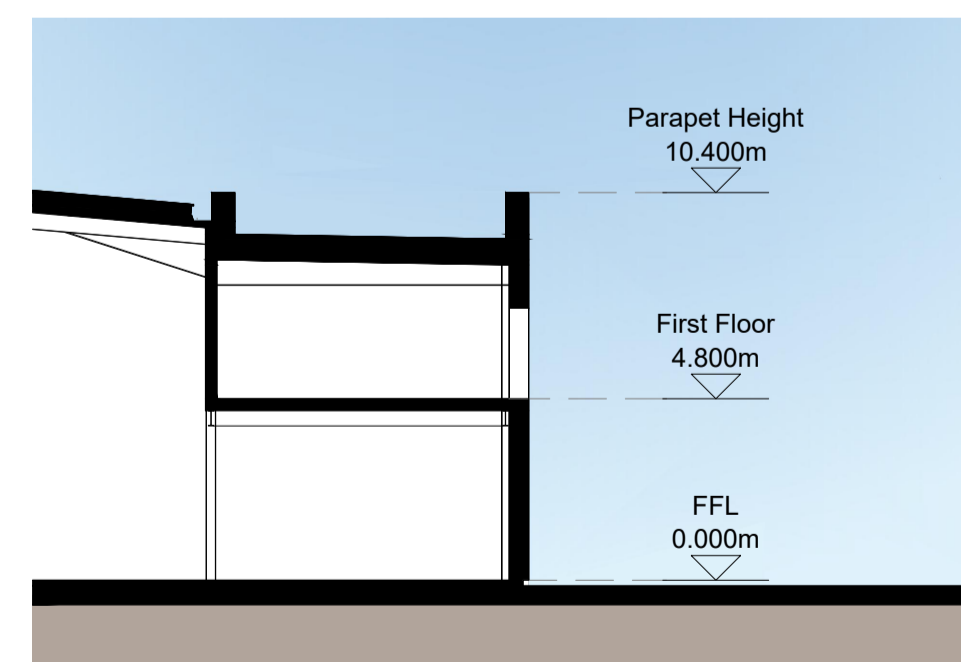
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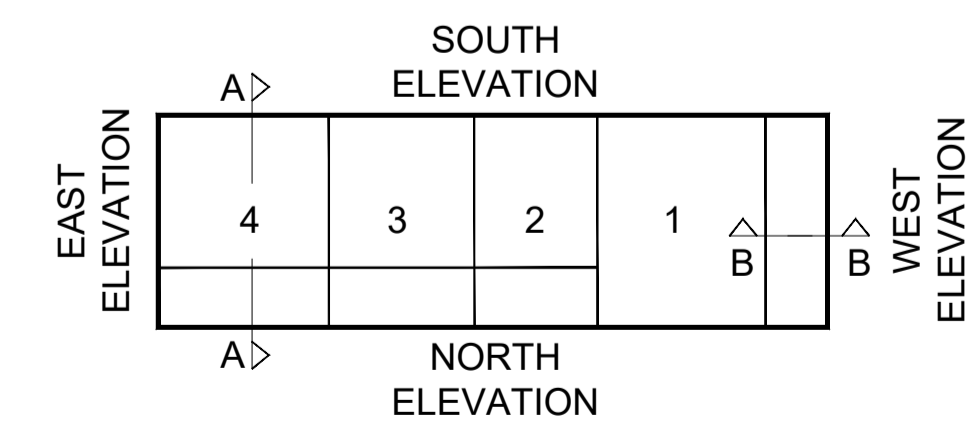
SOUTH ELEVATION



TYPICAL SECTION A-A



TYPICAL SECTION B-B



REFERENCE PLAN

- MATERIALS KEY**
- ① PROFILED ROOF CLADDING (GOOSEWING GREY RAL 7038)
 - ② EUROPEAN REDWOOD TIMBER CLADDING
 - ③ HORIZONTAL ARC PROFILE CLADDING (METALLIC SILVER RAL 9006)
 - ④ ALUMINIUM FRAMED WINDOWS WITH SECURITY SHUTTERS AT GROUND FLOOR (ANTHRACITE RAL 7016)
 - ⑤ ALUMINIUM FRAMED CURTAIN WALL GLAZING (ANTHRACITE RAL 7016)
 - ⑥ LOADING BAY DOORS (ANTHRACITE RAL 7016)
 - ⑦ STEEL PERSONNEL DOORS (ANTHRACITE RAL 7016)
 - ⑧ STEEL BOLLARD (TEAL RAL 5009)
 - ⑨ MAIN ENTRANCE BRISE-SOLEIL
 - ⑩ SIGNAGE ZONE, INDICATIVE
 - ⑪ BACKLIT ILLUMINATED ALUMINIUM UNIT NUMERAL (TEAL)
 - ⑫ ALUMINIUM FRAMED GLAZED ENTRANCE DOOR (ANTHRACITE RAL 7016)

Client:
HE2 UK ENTERPRISES 12 GP LIMITED (LONDON)



Project:
COMMERCIAL ROAD EDMONTON

12 Warren Yard,
Warren Park,
Milton Keynes,
MK12 5HW
01908 305 246

info@prc-group.com
www.prc-group.com

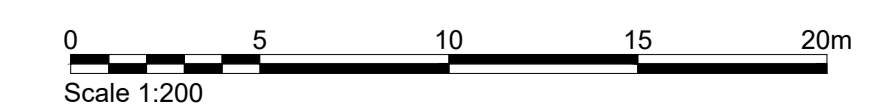
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Date: OCT 22

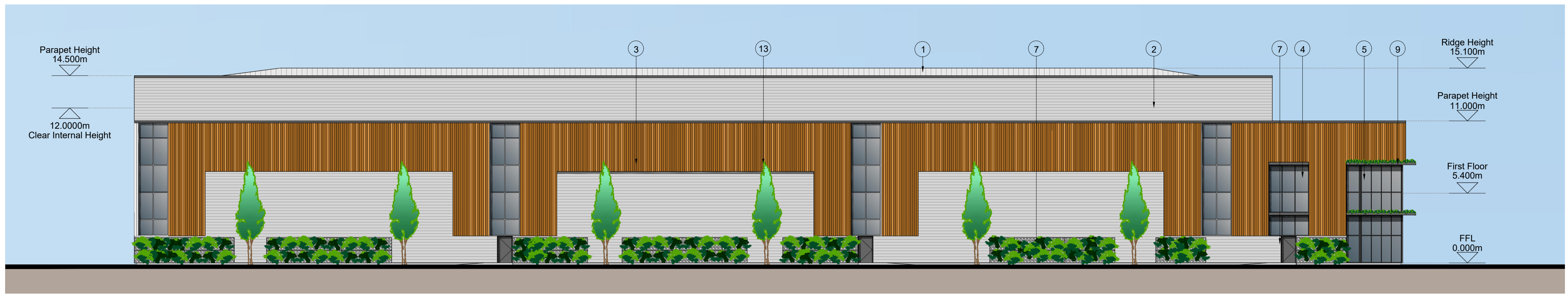
Job No: 11407
Stage: PL 014
Drawing No: Rev:

Issue Status:
Construction Preliminary
Information Approval
Tender

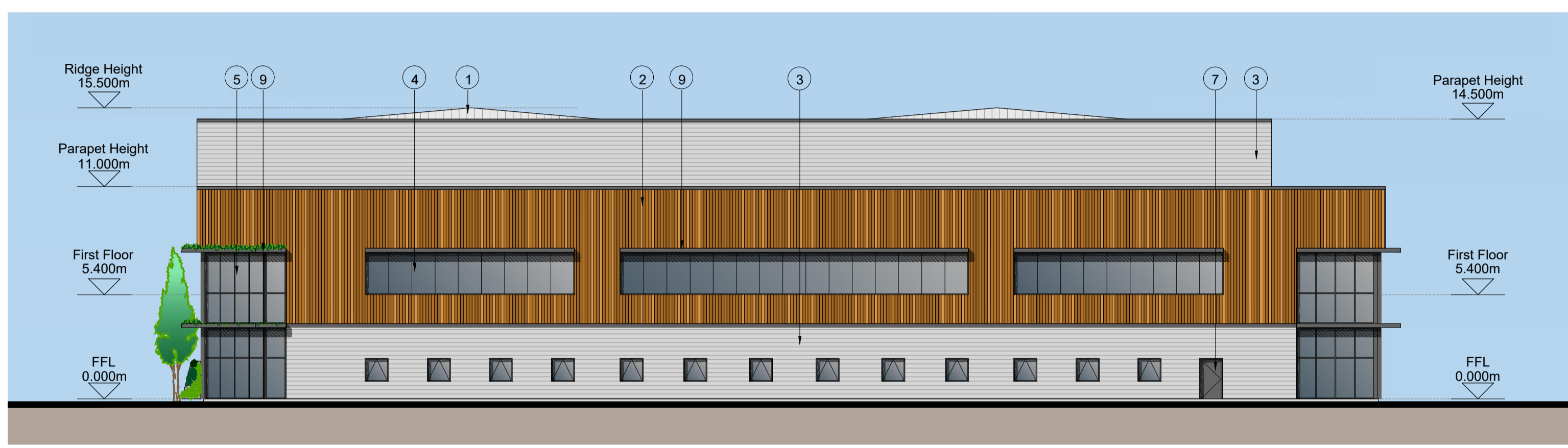
Offices
Working
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Milton Keynes
Warsaw



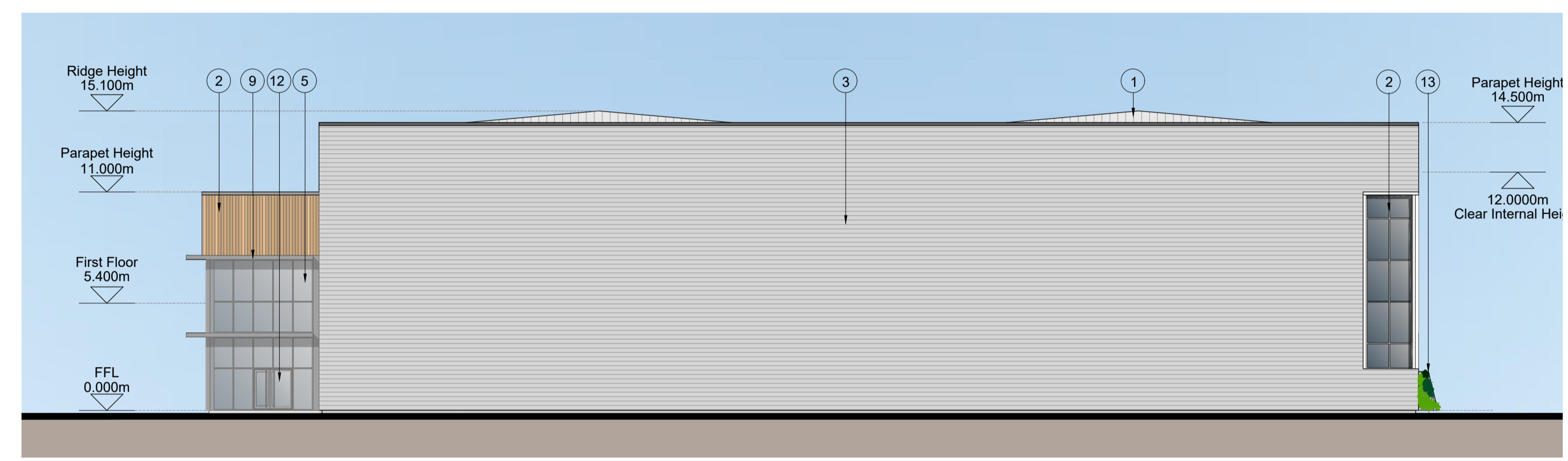
Revisions:	Drawn/Chkd:	Date:
A. Additional door added to south elevation	AMc	Dec 22
B. West elevation revised	AH	April 23
C. South West corner amended for cycle route	AH	April 23



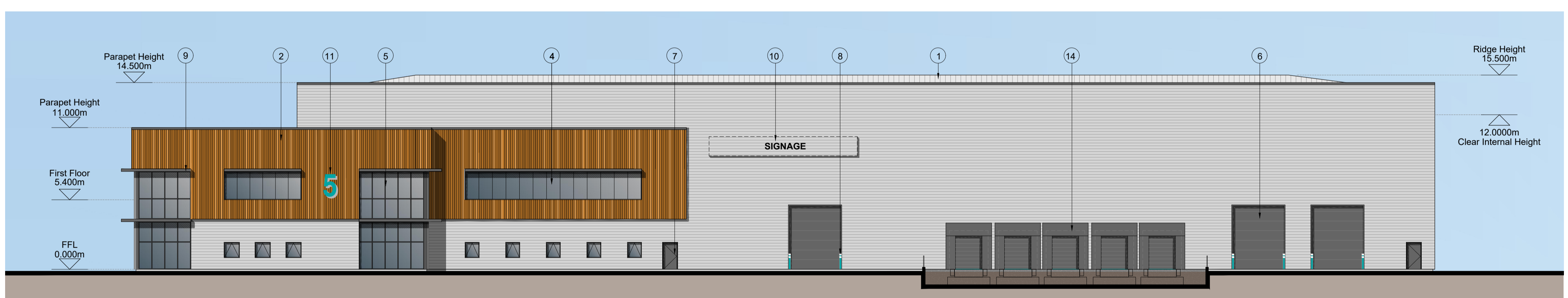
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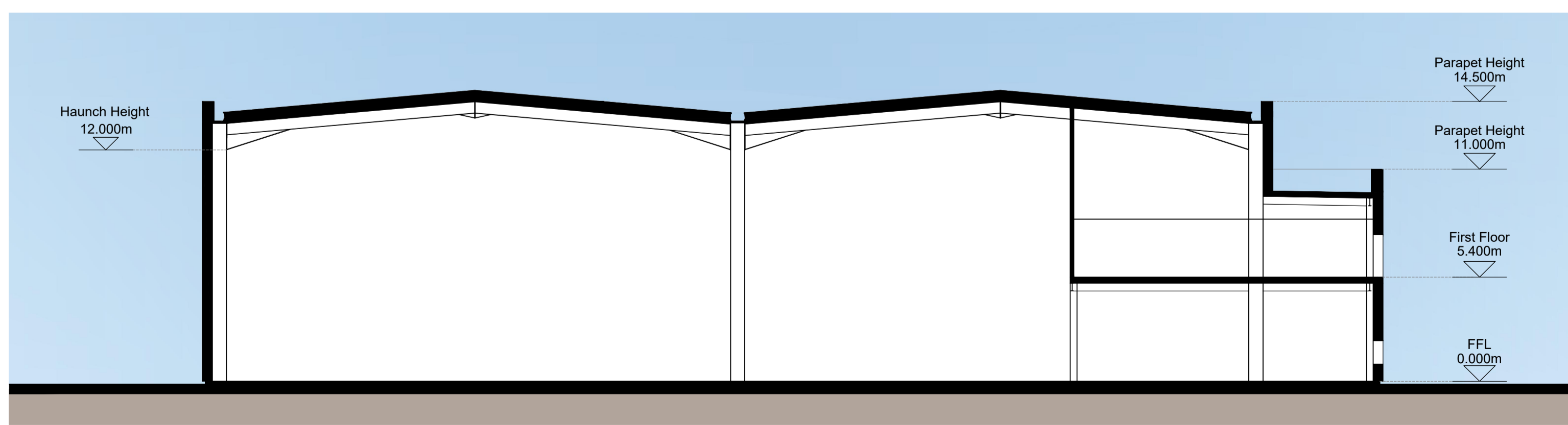
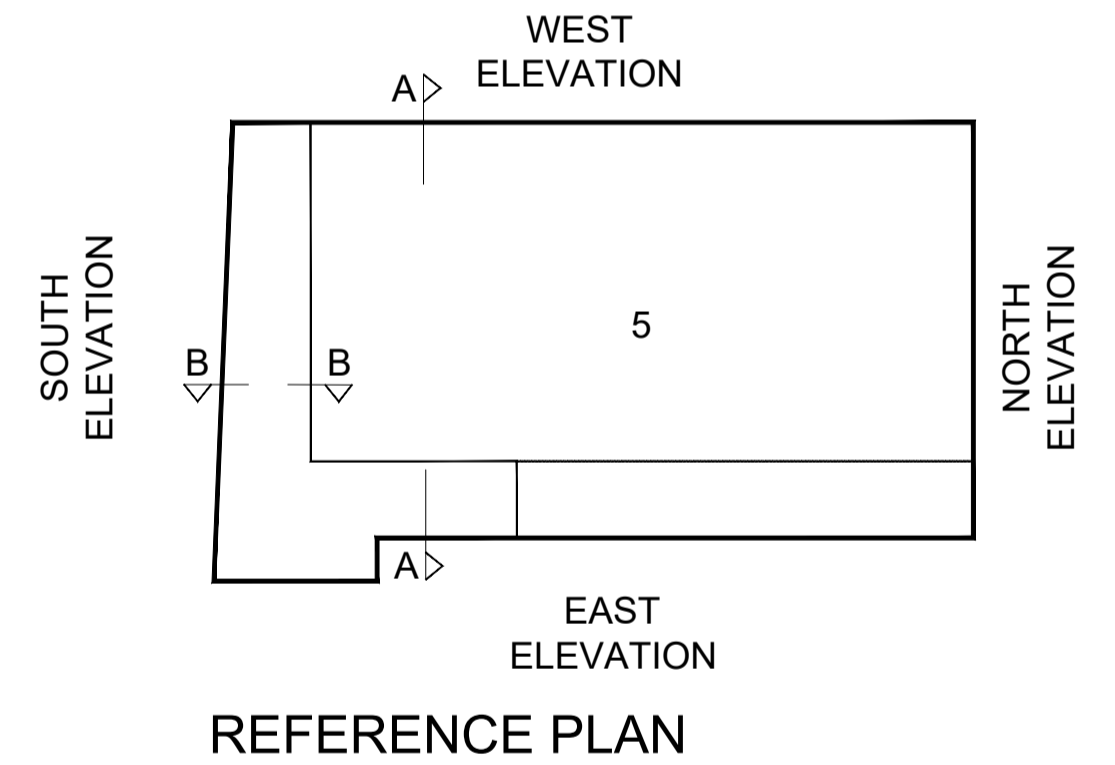
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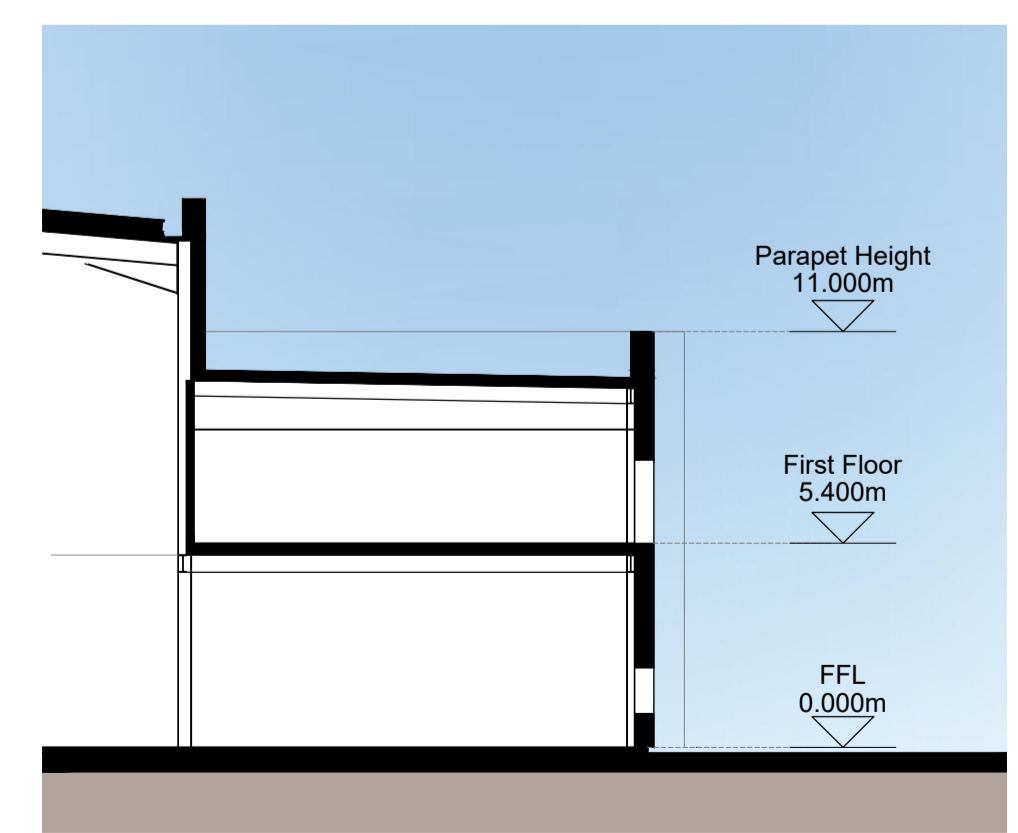
NORTH ELEVATION



EAST ELEVATION



TYPICAL SECTION A-A



TYPICAL SECTION B-B

MATERIALS KEY

- ① PROFILED ROOF CLADDING (GOOSEWING GREY RAL 7038)
- ② EUROPEAN REDWOOD TIMBER CLADDING
- ③ HORIZONTAL ARC PROFILE CLADDING (METALLIC SILVER RAL 9006)
- ④ ALUMINIUM FRAMED WINDOWS WITH SECURITY SHUTTERS AT GROUND FLOOR (ANTHRACITE RAL 7016)
- ⑤ ALUMINIUM FRAMED CURTAIN WALL GLAZING (ANTHRACITE RAL 7016)
- ⑥ LOADING BAY DOORS (ANTHRACITE RAL 7016)
- ⑦ STEEL PERSONNEL DOORS (ANTHRACITE RAL 7016)
- ⑧ STEEL BOLLARD (TEAL RAL 5009)
- ⑨ MAIN ENTRANCE BRISE-SOLEIL
- ⑩ SIGNAGE ZONE, INDICATIVE
- ⑪ BACKLIT ILLUMINATED ALUMINIUM UNIT NUMERAL (TEAL)
- ⑫ ALUMINIUM FRAMED GLAZED ENTRANCE DOOR (ANTHRACITE RAL 7016)
- ⑬ STONE GABIONS WITH PLANTING
- ⑭ DOCK LEVELLERS

Client:
HE2 UK ENTERPRISES 12 GP LIMITED (LONDON)



Project:
COMMERCIAL ROAD EDMONTON

12 Warren Yard, Warren Park, Milton Keynes, MK12 5NW
01908 305 246
info@prc-group.com
www.prc-group.com

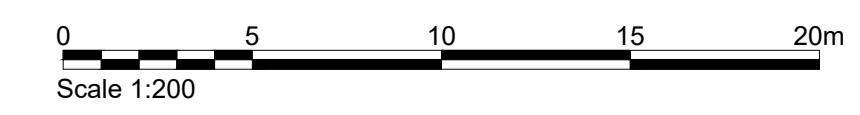
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Job No:	Stage:	Drawing No:	Rev:
11407	PL	015	C

Issue Status:
Construction Preliminary
Information Approval
Tender

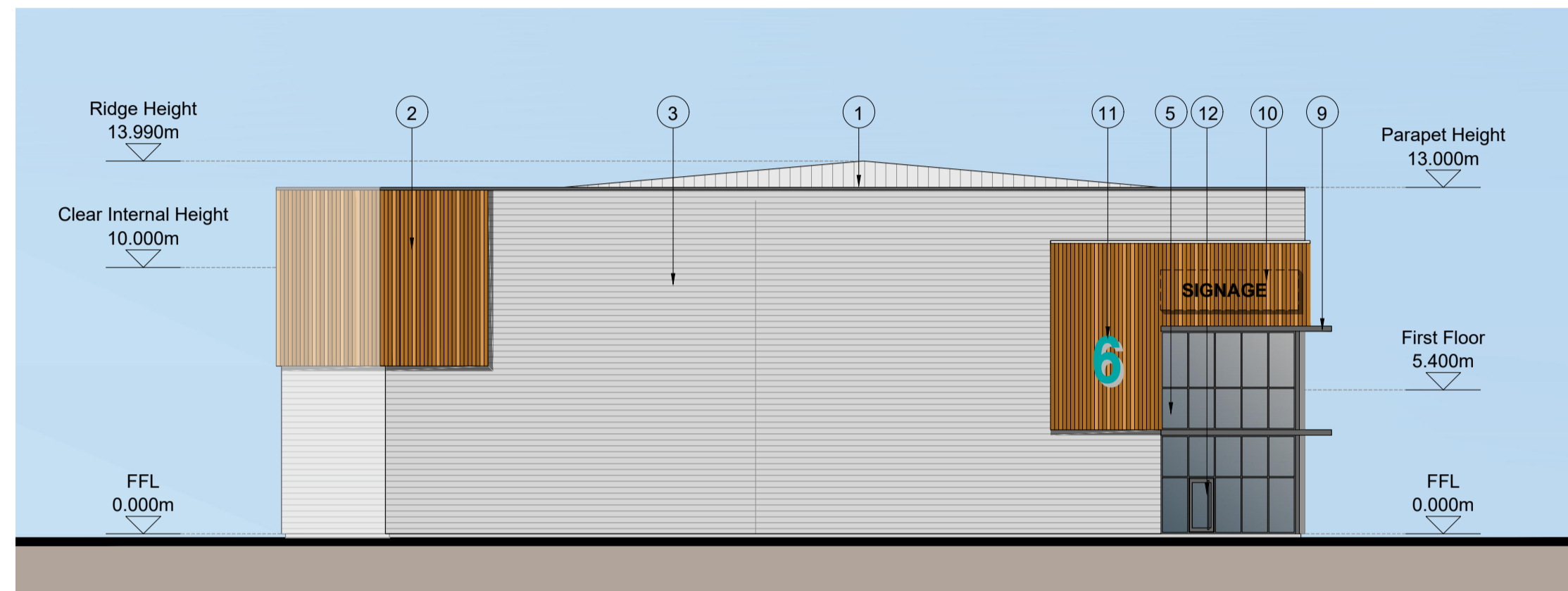
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Planning
Master Planning
Urban Design
Interiors
Landscape
Offices
Woking
London
Milton Keynes
Warsaw



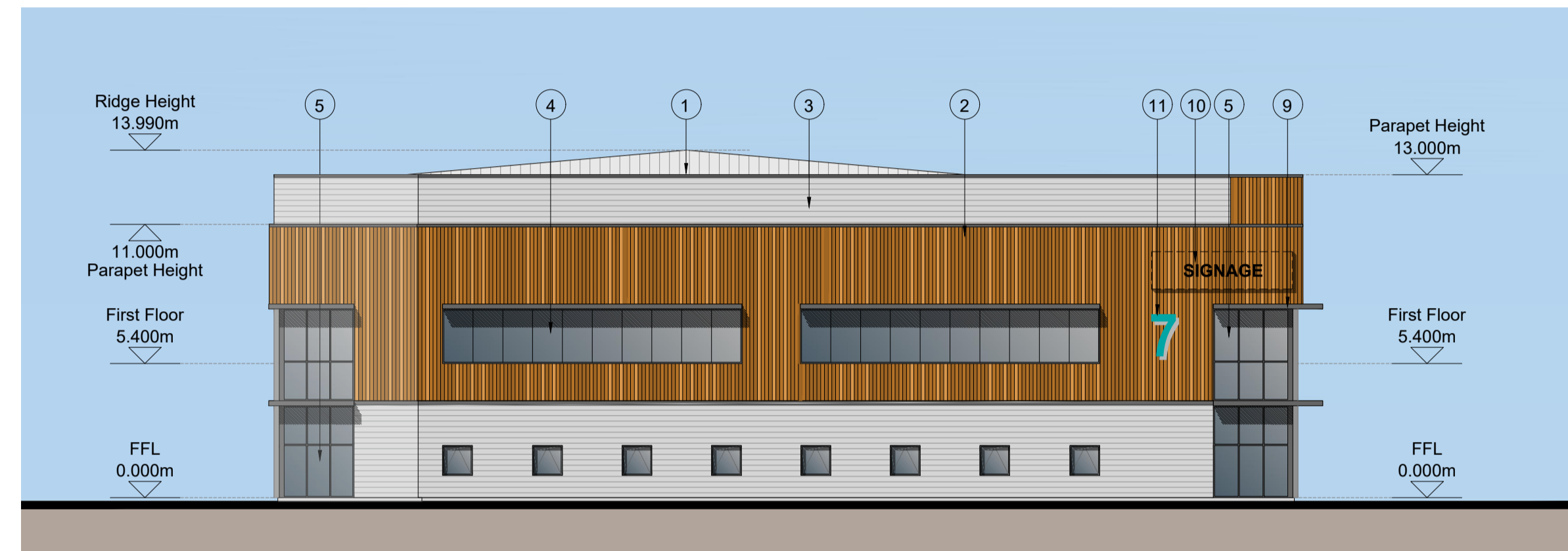
Revisions:	Drawn/Chkd:	Date:
A. Length of unit 6 amended	AH	April 23
B. Item 13 reference updated on key	EG	July 23



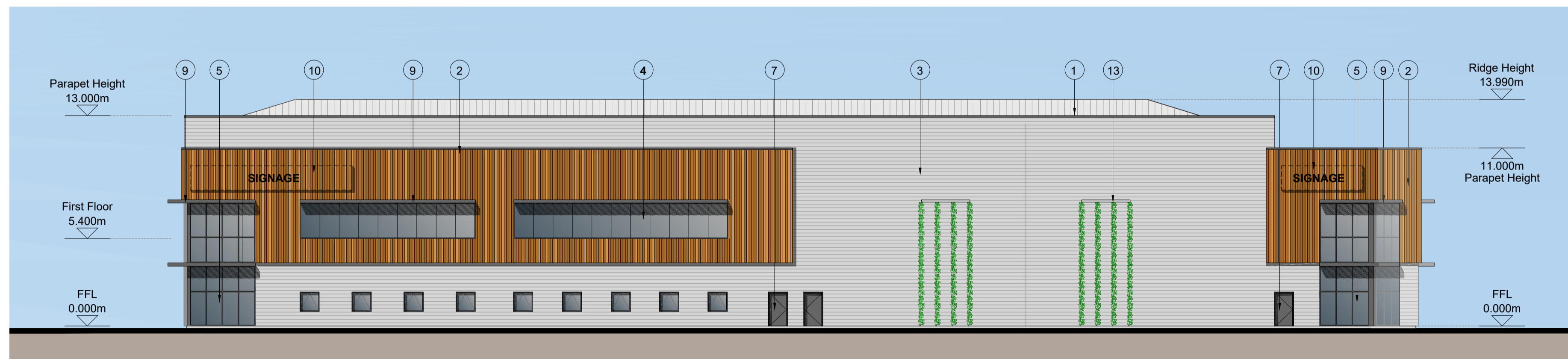
NORTH ELEVATION



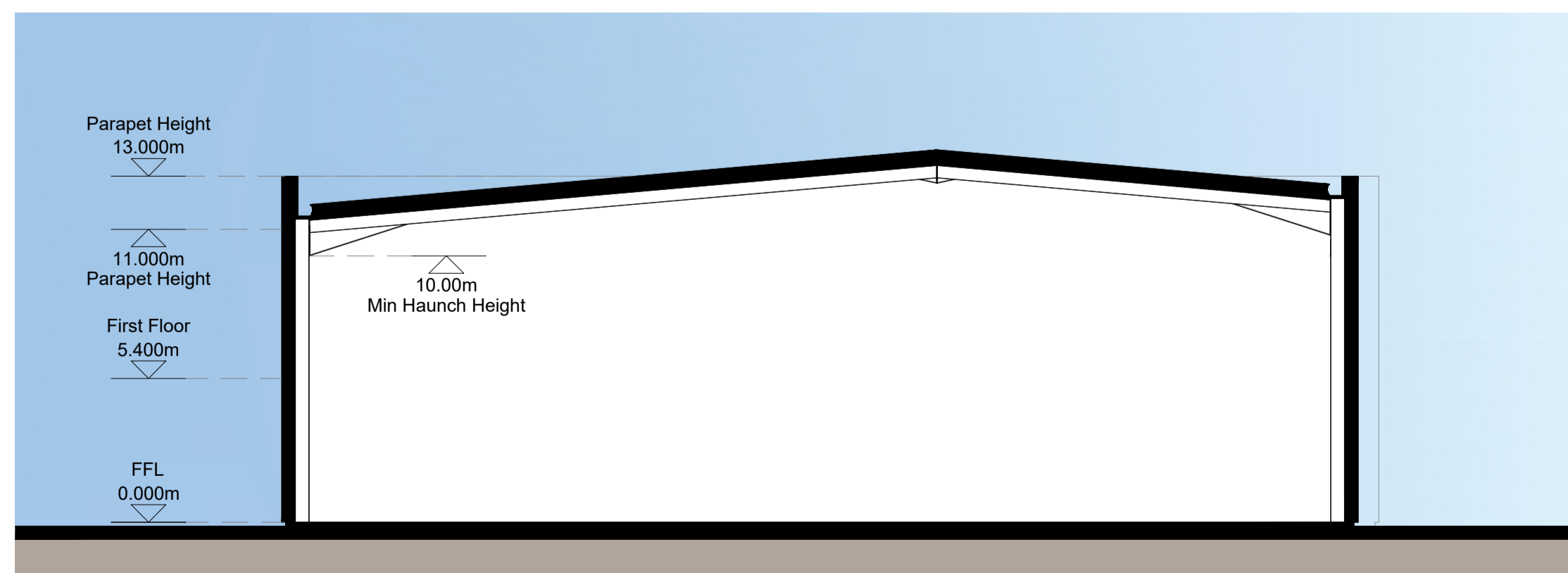
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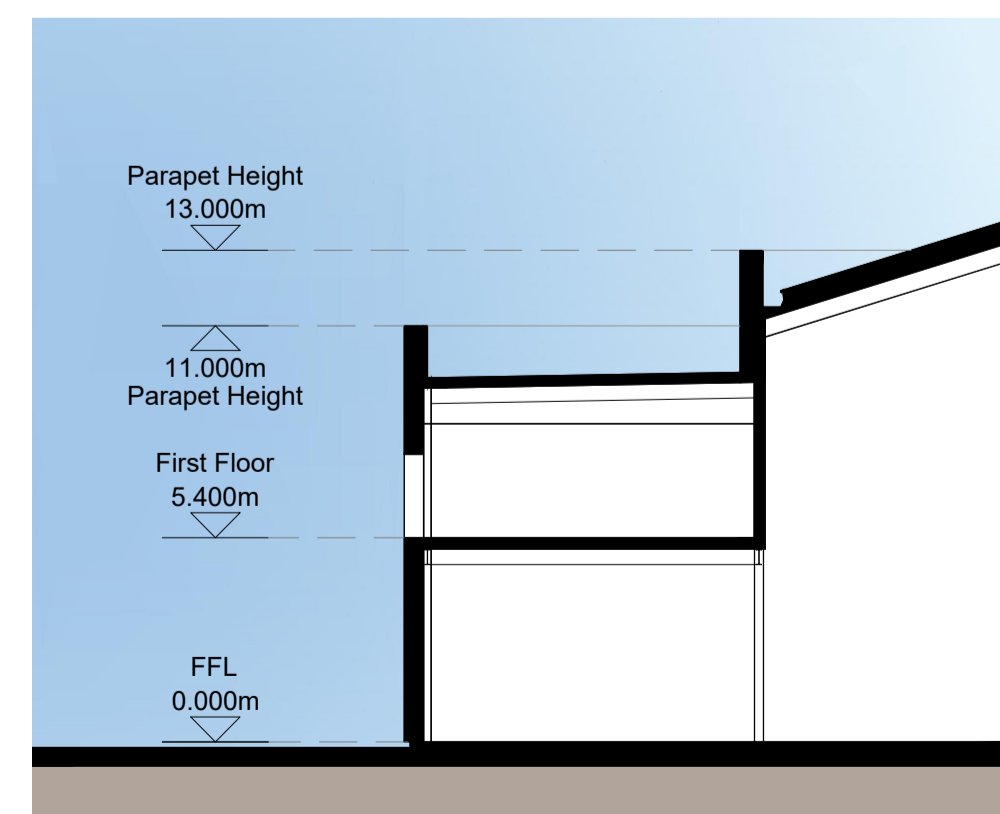
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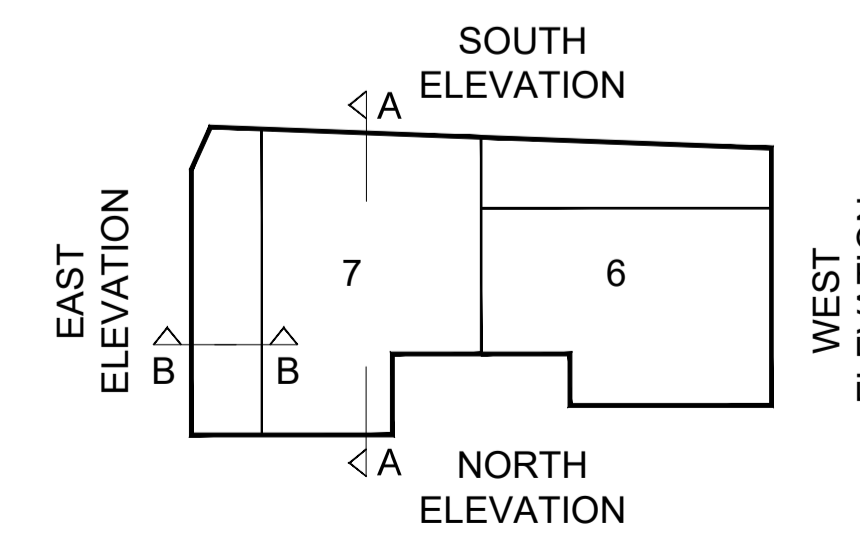
SOUTH ELEVATION



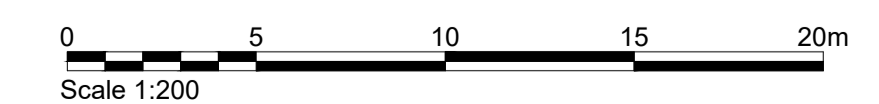
SECTION A-A



SECTION B-B



REFERENCE PLAN



- MATERIALS KEY**
- ① PROFILED ROOF CLADDING (GOOSEWING GREY RAL 7038)
 - ② EUROPEAN REDWOOD TIMBER CLADDING
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 - ⑨ MAIN ENTRANCE BRISE-SOLEIL
 - ⑩ SIGNAGE ZONE, INDICATIVE
 - ⑪ BACKLIT ILLUMINATED ALUMINIUM UNIT NUMERAL (TEAL)
 - ⑫ ALUMINIUM FRAMED GLAZED ENTRANCE DOOR (ANTHRACITE RAL 7016)
 - ⑬ GREEN LIVING WALL

Client:
HE2 UK ENTERPRISES 12 GP
LIMITED (LONDON)



Project:
COMMERCIAL ROAD
EDMONTON

12 Warren Yard,
Warren Park,
Milton Keynes,
MK12 5HW
01908 305 246

info@prc-group.com
www.prc-group.com

Drawing Title:
PROPOSED ELEVATIONS
UNIT 6-7

Scale @ A1:	Checked by:	Date:
1:200	AMc	OCT 22

Job No:	Stage:	Drawing No:	Rev:
11407	PL	016	B

Issue Status:	Architecture
Construction <input type="checkbox"/>	Planning <input type="checkbox"/>
Information <input type="checkbox"/>	Master Planning <input type="checkbox"/>
Tender <input type="checkbox"/>	Urban Design <input type="checkbox"/>
	Interiors <input type="checkbox"/>
	Landscape <input type="checkbox"/>

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